



AGENDA

DESIGN REVIEW COMMITTEE

May 5, 2016

5:30 p.m.

2nd Floor Council Chambers

1095 Duane Street ° Astoria OR 97103

1. CALL TO ORDER
2. INTRODUCTIONS
3. ROLL CALL
4. ELECTION OF OFFICERS
 - a. In accordance with Sections 1.110 and 1.115 of the Astoria Development Code, the DRC needs to elect officers for 2016. The 2015 officers were President Jared Rickenbach, Vice President LJ Gunderson, and Secretary Sherri Williams.
5. MINUTES
 - a. June 4, 2015
6. PUBLIC HEARINGS
 - a. Design Review DR16-01 by Josh Kolberg, Architect for PKA Architects on behalf of Columbia Lutheran Charities dba Columbia Memorial Hospital to construct a approximate 19,657 square foot, two story building dedicated to cancer related treatment and services at 1905 Exchange within the Gateway Area in the AH-HC, Attached Housing - Health Care zone.
7. REPORT OF OFFICERS
8. NEW BUSINESS
 - a. New Committee Member List – For Information Only
9. PUBLIC COMMENTS (Non-Agenda Items)
10. ADJOURNMENT

THIS MEETING IS ACCESSIBLE TO THE DISABLED. AN INTERPRETER FOR THE HEARING IMPAIRED MAY BE REQUESTED UNDER THE TERMS OF ORS 192.630 BY CONTACTING SHERRI WILLIAMS, COMMUNITY DEVELOPMENT DEPARTMENT, 503-338-5183.

DESIGN REVIEW COMMITTEE
Astoria City Hall
June 4, 2015

CALL TO ORDER:

President Rickenbach called the meeting to order at 5:30 p.m.

ROLL CALL – ITEM 2:

Commissioners Present: President Jared Rickenbach, Vice President LJ Gunderson, Paul Tuter, and Hilarie Phelps.

Commissioners Excused: Derith Andrew

Staff Present: Interim Planner Mike Morgan.

The Design Review Committee proceeded to Item 4(a): DR15-03.

APPROVAL OF MINUTES – ITEM 3:

This item was addressed immediately following Item 4(a): DR15-03.

President Rickenbach called for approval of the minutes of the April 7, 2015 meeting. Commissioner Phelps moved to approve the April 7, 2015 minutes as presented; seconded by Commissioner Tuter. Motion passed unanimously.

The Design Review Committee proceeded to Item 5: Reports of Officers and Commissioners.

PUBLIC HEARINGS:

President Rickenbach explained the procedures governing the conduct of public hearings to the audience and advised that the substantive review criteria were available from Staff.

ITEM 4(a):

DR15-03 Design Review DR15-03 by Wesley Houck for Steven and Cheryl Mazzuca to construct a 2,271 square foot single family dwelling at 275 29th Street within the Gateway Area in the AH-MP, Attached Housing-Mill Pond zone.

This Item was addressed immediately following Item 2: Roll Call.

President Rickenbach asked if anyone objected to the jurisdiction of the Design Review Committee to hear this matter at this time. There were no objections. He asked if any member of the Design Review Committee had any conflicts of interest or ex parte contacts to declare. Hearing none, he called for a presentation of the Staff report.

Interim Planner Morgan reviewed the Findings and Conditions contained in the Staff report, noting that the project has not yet been reviewed by the Mill Pond Homeowners Association. No correspondence had been received and Staff recommended approval with conditions.

Vice President Gunderson asked if garden windows were encouraged or discouraged. Interim Planner Morgan said the punched out widows have been allowed in several Mill Pond houses and are not discouraged by the City.

President Rickenbach opened the public hearing and called for testimony from the Applicant.

Wesley Houck, 835 Avenue S, Suite E, Seaside, said he was available to answer questions.

President Rickenbach called for testimony in favor of, impartial, or opposed to the application.

Helen Westbrook, 2860 Log Bronc Way, Astoria, Mill Pond Homeowners Association Architectural Review Committee Chair, spoke impartial to the application. She said the homeowners association (HOA) did not have a complete application for this project. She appreciated Vice President Gunderson's question about the garden windows and explained that the HOA approved a garden window on a home currently under construction only because the window will be on the back of the house and sheltered by another part of the building. The garden window was included in the design after the Design Review Committee reviewed and approved the application for that house. She believed one other house in Mill Pond had a similar window, but could not recall which house.

President Rickenbach confirmed there was no further public comment or closing remarks from Staff and closed the public hearing. He called for Committee discussion and deliberation.

Commissioner Phelps said she assumed projects were approved by the HOAs prior to review by the Design Review Committee. She asked if the Committee would review the project a second time if the HOA requires changes. Interim Planner Morgan confirmed only major modifications would need to be reviewed by the Design Review Committee. Minor changes can be approved administratively. The City cannot rely on the HOA's decision; however, if the HOA does not approve of the project, construction would not be allowed.

President Rickenbach believed the 5½-inch by ¾-inch window trim would not be appropriate with the type of siding that has been proposed. Interim Planner Morgan clarified the trim size in the Staff report may have included a mistake.

Mr. Houck said he assumed the trim would be 5½-inch by 5/4-inch, noting that he had never used ¾-inch trim.

President Rickenbach noted the Staff report stated several times that the window trim would be 5½-inch by ¾-inch. It is important that the siding does not protrude past the face of the trim. Therefore, the Staff report needs to be corrected.

Vice President Gunderson said the garden window on the existing Mill Pond home sticks out and she believed it did not fit the look of the neighborhood. While the window is on the back side of the house, it faces the Gateway area. The Staff report states the garden window on the proposed house would be on the rear portion of the main structure; however, there is no indication of the window on the elevations.

Mr. Houck confirmed no garden window was proposed for this house.

President Rickenbach noted the Staff report would need to be amended to reflect this. Interim Planner Morgan noted the language is likely carried over from a previous report.

Vice President Gunderson moved the Astoria Design Review Committee adopt the Findings and Conclusions contained in the Staff report and approve Design Review DR15-03 by Wesley Houck for Steven and Cheryl Mazzuca to construct a 2,271 square foot single family dwelling at 275 29th Street with the recommended conditions and the following corrections to the Staff report:

- Page 5, Section F: Strike the fifth sentence under Finding, which states, "There is a garden window on the rear portion of the main structure."
- Page 5, Section F, second paragraph under Finding: "Window and door exterior casings proposed to be 5.5" x ¾" 1" or larger..."
- Page 6, Section G, fourth sentence under Finding: "There would be 5.5" x ¾" 1" corner boards."

Motion seconded by Commissioner Phelps. Motion passed unanimously. Ayes: President Rickenbach, Vice President Gunderson, Commissioners Phelps and Tuter. Nays: None.

President Rickenbach read the rules of appeal into the record.

The DRC proceeded to Item 3: Approval of Minutes.

REPORTS OF OFFICERS/COMMISSIONERS – ITEM 5:

This item was addressed immediately following Item 3: Approval of Minutes.

President Rickenbach asked Staff to find out if garden windows were encouraged or discouraged. Interim Planner Morgan noted the garden window on the existing home was not approved during his tenure.

Commissioners discussed the difference between bay windows and garden windows. The Commission and Staff discussed how the existing house ended up with the garden window, which was approved as part of a design change by former Planner Rosemary Johnson. Interim Planner Morgan said he would review the entire application packet to verify that the window was approved appropriately.

Vice President Gunderson said the view of the garden window from the Gateway area looked out of place. Interim Planner Morgan noted the view of the window would be blocked after the apartment complex on the vacant lot is constructed.

ADJOURNMENT:

There being no further business, the meeting was adjourned at 5:53 p.m.

ATTEST:

APPROVED:

Secretary

Interim Planner

STAFF REPORT AND FINDINGS OF FACT

April 29, 2016

TO: DESIGN REVIEW COMMITTEE

FROM: NANCY FERBER, PLANNER *Nancy Ferber*

SUBJECT: DESIGN REVIEW REQUEST (DR16-01) BY PKA ARCHITECTS ON BEHALF OF COLUMBIA LUTHERAN CHARAITIES (COLUMBIA MEMORIAL HOSPITAL) TO CONSTRUCT A 19,657 SQUARE FOOT TWO-STORY CANCER CLINIC FACILITY AT 1905 EXCHANGE STREET

I. BACKGROUND SUMMARY

- A. Applicant: PKA Architects (Josh Kolberg)q
on behalf of Columbia Memorial Hospital (CMH)
6969 SW Hampton Street
Portland OR 97223
- B. Owner: Columbia Lutheran Charities dba Columbia Memorial Hospital
72111 Exchange Street
Astoria, OR 97103
- C. Location: 1905 Exchange Street; Map T8N-R9W Section 8DC and 8DD,
Tax Lot 1-12; Blocks 111 & 138, Shively
- D. Zone: AH-HC Zone (Attached Housing – Health Care)
- E. Proposal: To construct a two-story building dedicate to cancer related
treatment and services for the north coast community
- F. Previous Applications N/A-New construction for this site

II. BACKGROUND

Site:

The subject property is located on the south side of Exchange Street, on the corner of 19th and Exchange Street within the Gateway Overlay District. The proposed cancer center will be located at the west end of



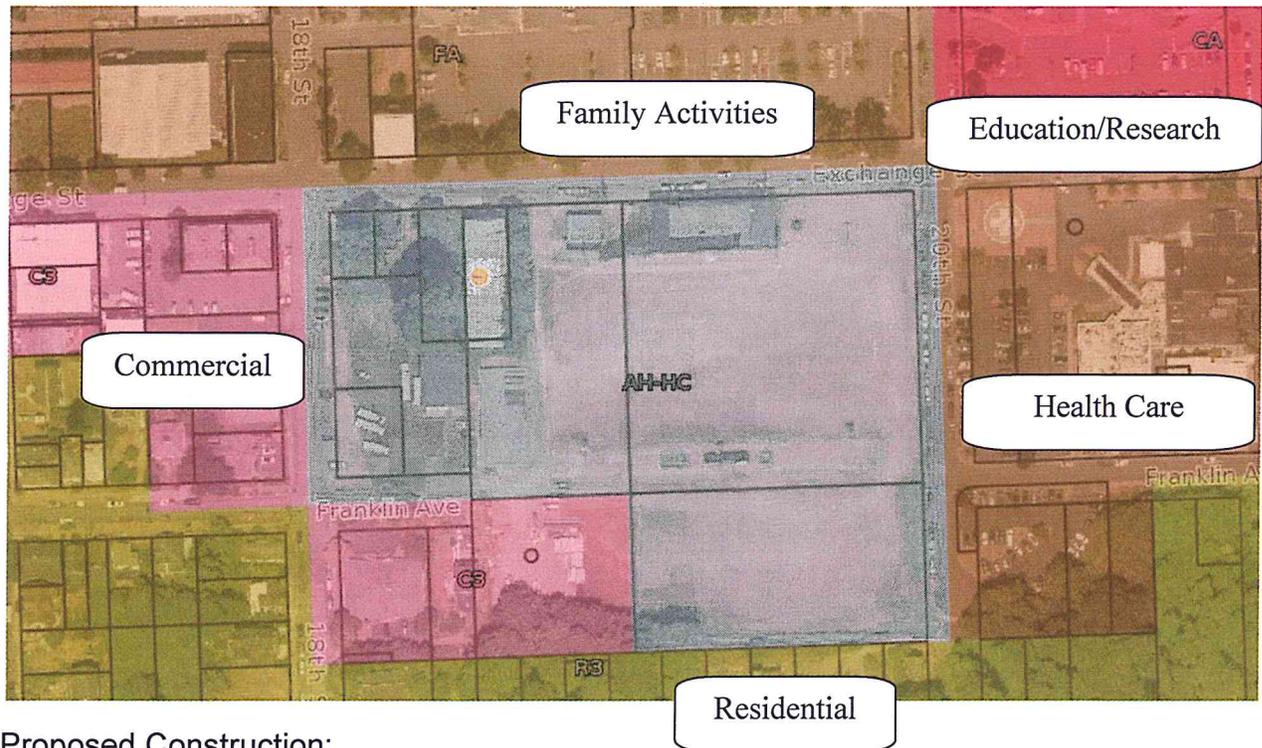
the existing Columbia Memorial Hospital (CMH). The project covers multiple lots, and a large portion of the area that was once the John Warren field. The proposed area encompasses approximately 91,000 square feet.

The site has been vacant since the Warren field was purchased. The bleachers have since been removed, and recent parking lot improvements were completed to the east of the site.



Area:

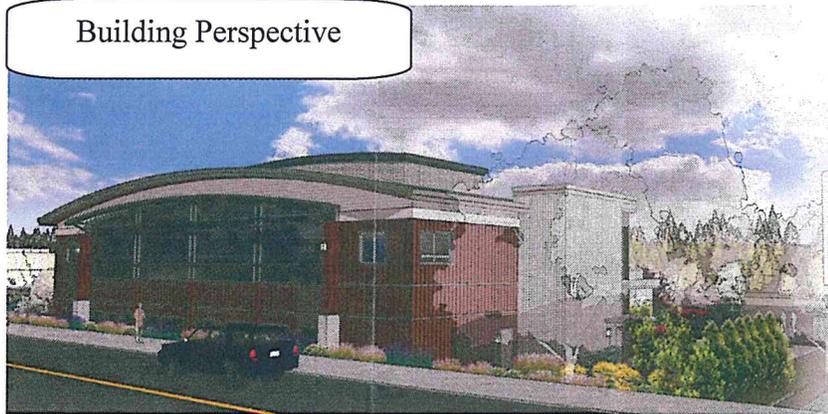
The area is developed with a mixture of residences to the south across Franklin Ave toward Grand. Education and Health Care facilities are to the east, commercial area to the west and family activities such as the cinema to the north. The building will be oriented along Exchange Street between 18th and 19th, east of the Yost Property. The main driveway entry will be along Exchange Street, with building off the patient parking in the back of the site.



Proposed Construction:

This proposal is to construct a two-story facility for cancer care and treatment. Oncology services at the existing CMH have increased to a full medical clinic with an infusion center. This project expands the CMH campus to include the proposed

Building Perspective



building which will be 31 feet, 3 inches in height excluding a mechanical penthouse. This project would include 25% landscaped spaces, exceeding the 20% minimum requirement. A traffic study and geological investigation have been included in the appendix of the application. Portions of the proposed perspectives are referenced in the staff report. Refer to the full plans submitted for more details.

Style: Two-story square shaped care facility with barrel vaulted roof. First floor proposed square footage is 10,479, the second floor square footage is 9,178. A parking lot is proposed behind the building.

Roof: Silver and gray barrel vaulted over the main building, with low profile standing seam; mixture of steel and EPDM membrane with curved steel brackets over windows on the second story north elevation. Flat EPDM roofing is proposed over the entry lobby, entrance canopy and west exit staircase. The roof system matches the roof type for the CMH Pavilion building.

Siding: Vertical metal siding, with vertical board and batten fiber cement siding comprised of 3 inch battens and 12 inch boards. Horizontal fiber cement lap siding, and painted metal fascia boards and soffits; wood under the awning paneling and exposed concrete at the therapy vault.

Windows: Anodized aluminum storefront system; possible curtainwall on the north elevation, all window areas are true divided lites, windows are not operable, sections are 2" wide and 4" deep. Glazing is proposed for the north elevation to provide privacy for patients, and avoid glare from the large expanse of window. No smoked or mirrored glazing would be used, but there is a slight reflectivity or low-e glazing system proposed.

Doors: Entry door will be anodized aluminum, automatic sliding door. The south entry/exit from the classroom, and the west exit door is a proposed anodized alum swing door. Door are similar to the existing doors at the CMH Pavilion.

Other Design Elements: Landscaping incorporates storm water swale planting, a rain celebration feature and garden with seating area, and bike lockers. All trim and

weatherproofing detailing will be similar to the CMH Pavilion Building. Additional decorative elements include wooden eave with canopy ceiling, and decorative wood columns for canopy support on the north elevation. Wall treatments will be similar to the existing Pavilion building including galvanized metal panels and some exposed concrete.

Exterior Lighting: Exterior decorative and parking lot lighting will be included, it will be faced downward to avoid casting a glare. Parking lot lighting will match the parking lot lighting style in the recently completed lot (fall 2015).

Sign: The proposed development will include informational signs such as hours of operation, less than 1 square foot, and open/closed signs less than 1.5 square feet, which are exempt signs per code in Article 8.040. One monument sign and one building sign is proposed. Dimensions and materials shall be submitted with the sign permit pending a CMH and OHSU agreement on a name for the building.

Trash Enclosure: A trash enclosure is proposed, no details were submitted. A site plan shall be submitted at the time of the building permit application for review.

III. PUBLIC REVIEW AND COMMENT

A public notice was mailed to all property owners within 250 feet pursuant to Section 9.020 on April 14, 2016. A notice of public hearing was published in the *Daily Astorian* on April 8, 2016. Any comments received will be made available at the Design Review Committee (DRC) meeting.

IV. APPLICABLE REVIEW CRITERIA AND FINDINGS OF FACT

- A. Section 14.015(A) states that in addition to conformance with the specific uses and standards of the individual zones, the following zones shall conform to the general regulations of the Gateway Overlay Zone in Sections 14.005 through 14.030. 1) Maritime Heritage; 2) Family Activities; 3) Attached Housing-Health Care; 4) Health Care; 5) Education/Research/ Health Care Campus; 6) Hospitality/Recreation; 7) Local Service; and 8) Attached Housing-Mill Pond.

Finding: The site of the proposed treatment facility would be located in the Attached Health Care Zone (AH-HC) and shall conform to Sections 14.005 through 14.030 of the Development Code. This criteria is met.

- B. Section 14.015(B) requires that each public or private development proposal within the Gateway Overlay Zone will be reviewed for consistency with the Design Review Guidelines in Sections 14.020 through 14.030.

Finding: The proposed treatment facility is a private development to be constructed within the Gateway Overlay Zone and as such will be reviewed for consistency with the Design Review Guidelines. This criteria is met.

- C. Section 14.020 states that the Design Review Guidelines shall apply to all new construction or major renovation. The guidelines are intended to provide fundamental principles that will assist in the review of the proposed development. The principles identify both “encouraged” and “discouraged” architectural elements. They are broad design objectives and are not to be construed as prescriptive standards.

Finding: The structure is new construction and as such is subject to the Design Review Guidelines. This criteria is met.

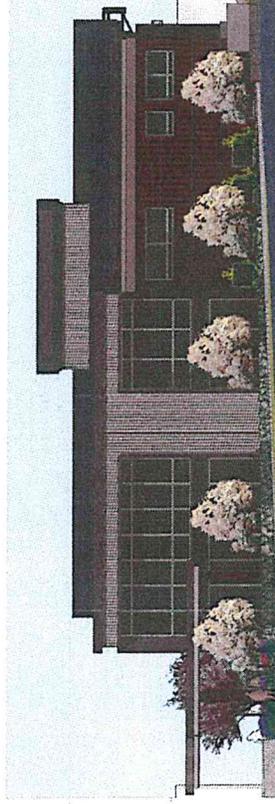
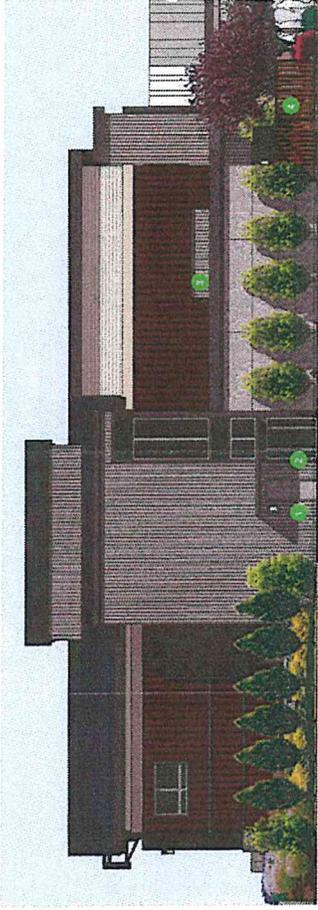
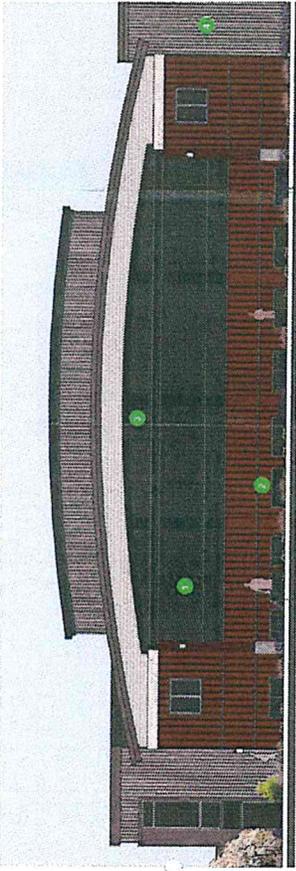
- D. Section 14.025(A) describes the purpose of the Design Review Guidelines and states that the Gateway Plan encourages new construction to reflect building types found in the Uppertown area. Three historic building types commonly found in the area include waterfront industrial, commercial, and residential.

Finding: The proposed development is commercial/medical, and at two-stories is low in form, which is an encouraged building type. The structure reflects designs found in the adjacent CMH Pavilion. The new building is simple with a general square plan shape, offset at the entry corner to help identify the building entrance. It is neither complex nor sprawling and maintains a compact design in character with the medical facilities in the area. None of the discouraged window types are incorporated. The proposed horizontal siding is an encouraged wall treatment. The roofing matches the Pavilion building, with a low slope barrel vault and incorporates a parapet wall on the flat roof area.

See building form photos on following page.

The overall design is reflective of a medical facilities in the area. The general design of the structure meets this guideline.

Building Form



E. Section 14.025(B) identifies the building forms encouraged.

1. All Building Types: a) Simple designs without extraneous details; b) Rectangular in plan; c) Square in plan.
2. Waterfront Industrial: a) Low in form; b) Cubic in form.
3. Commercial: a) Low in form.
4. Residential: a) Vertical in form; b) Cubic in form; c) Full front porch or front porch large enough to accommodate several seated persons.

Section 14.025(C) identifies the building forms discouraged.

1. All Building Types: a) Complex building footprints; b) Sprawling structures.

Finding: The treatment facility building would be square shaped with parking in the rear. The building footprint is not complex, nor sprawling. The building has a covered entryway for patients. This guideline is met.

F. Section 14.025(D) identifies the windows encouraged.

1. All Building Types: a) True-divided, multiple-light windows; b) Single-light windows; c) Applied muntins with profile facing window exterior; d) Rectangular windows with vertical proportions; e) Fixed windows; f) Double or single-hung windows; g) Casement windows; h) Windows should be spaced and sized so that wall area is not exceeded by window area, with the exception of commercial storefronts.
2. Waterfront Industrial: a) Square or rectangular windows with multiple lights.
3. Commercial: a) Storefronts: 1) Plate glass windows with multiple-light transom windows above; 2) Recessed entries; 3) Window to wall surface proportions may be exceeded; b) Upper Stories: 1) Window area should not exceed wall area.
4. Residential a) Vertical rectangle or square windows; b) Combination of single and multiple-light windows; c) Single windows, paired windows, or windows grouped in threes; d) Bay windows; e) Arched or decorative shaped windows used sparingly; f) Windows should use casings and crown moldings.

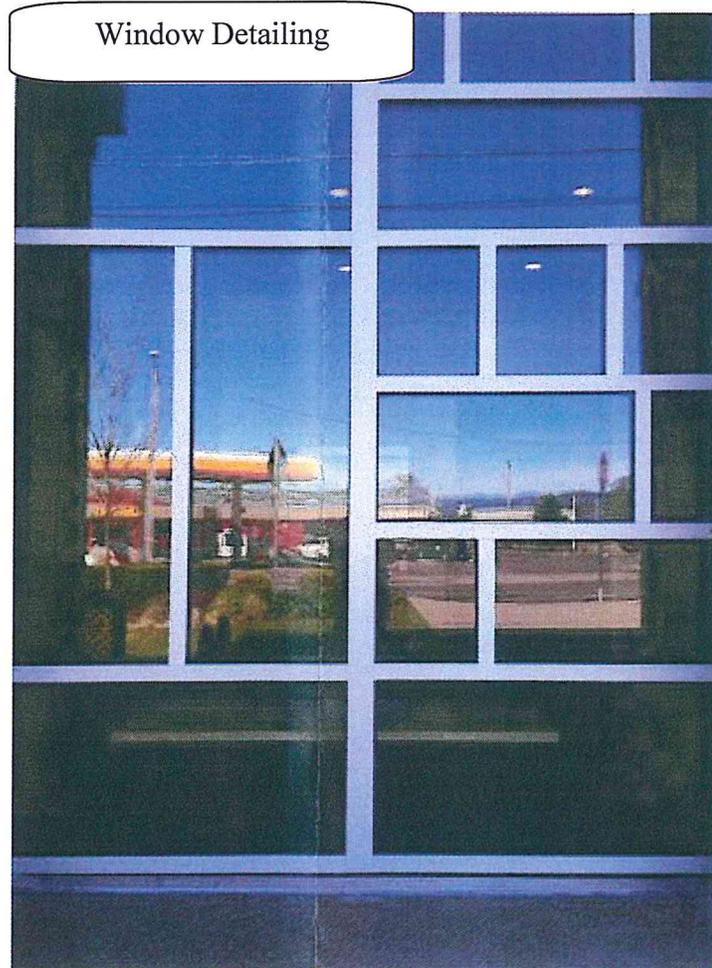
Section 14.025(E) identifies windows discouraged.

1. All Building Types: a) Applied muntins which have no profile; b) Smoked glass; c) Mirrored glass; d) Horizontal sliding windows; e) Walls predominated by large expanses of glass, except in commercial storefronts; f) Windowless walls. Large expanses of blank walls should only be located in areas which are not visible to the public; g) Aluminum frame windows, except in commercial storefronts.

Finding: Proposed windows are true-divided with multiple lites. Most will be aluminum storefront with a possible curtain wall system on the north elevation to provide river views to patients. The large expanse of glazed window intends to bring natural light into the patient care space and provide privacy. Windows shall be spaced and sized so that window area does not exceed wall area.

However if the north frontage is considered as a commercial façade the large window area is allowable.

There are windows on all elevations, there are no blank walls. A concrete wall on the west elevation is required as a barrier for radiation from a linear accelerator machine. There are narrow windows in the adjacent staff entry/exit area.



The main entry door is proposed anodized aluminum. The south door to the classroom is an anodized alum swing door, and the west door exiting from a corridor is an anodized swing door; both are similar to the neighboring Pavilion Building

The proposed windows and doors meet this guideline.

G. Section 14.025(F) identifies exterior wall treatments encouraged.

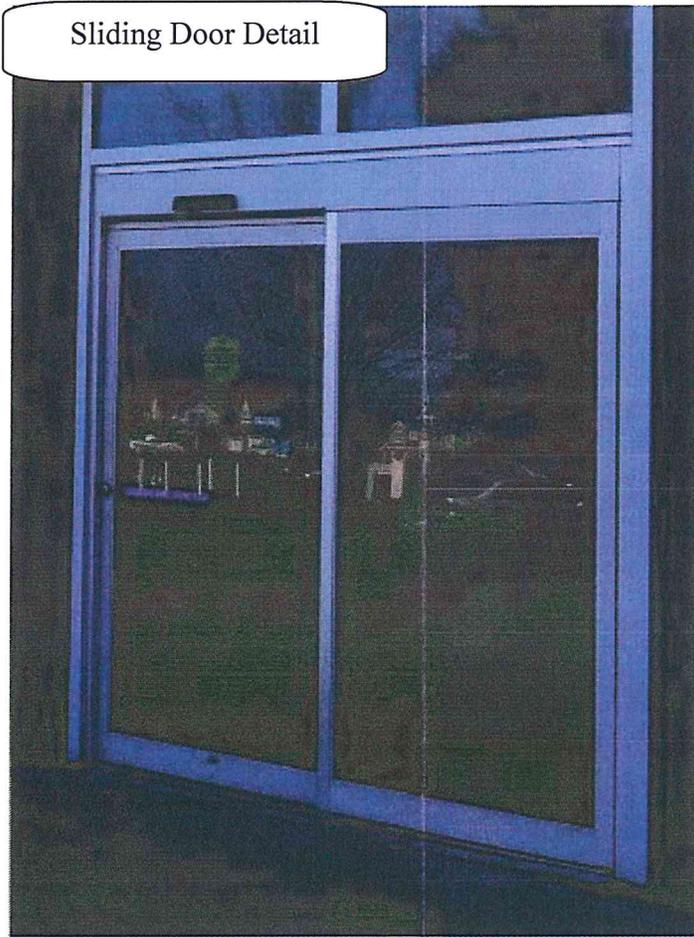
1. All Building Types: a) Drop siding; b) Weatherboard siding; c) Horizontal siding with six inches or less exposure.
2. Waterfront Industrial: a) Board and batten style; b) Galvanized corrugated metal.
3. Commercial: a) Finished concrete; b) Brick veneer.
4. Residential: a) Clapboard; b) Wood shingle (rectangular); c) Decorative wood shingle.

Section 14.025(G) identifies exterior wall treatments discouraged.

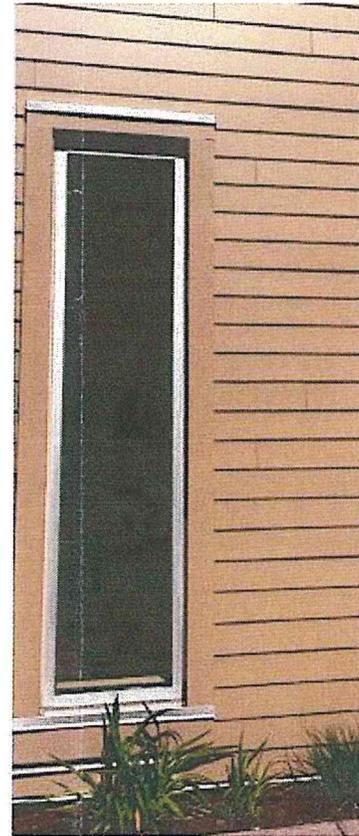
1. All Building Types: a) Exposed textured, concrete block; b) Flagstone or other applied stone products; c) Precast concrete or decorative concrete panels; d) Wood shakes; e) Plywood paneling.

Finding: The structure is proposed to be sided with a mixture of vertical board and batten fiber cement siding with 3" batten and 12" board. The horizontal siding proposed is fiber cement lap siding, similar to the Pavilion Building and

Sliding Door Detail



with a belt course would separate the siding styles for contrast as seen in the example below. This guideline is met.



Examples of proposed siding materials

H. Section 14.025(H) identifies the roof elements encouraged.

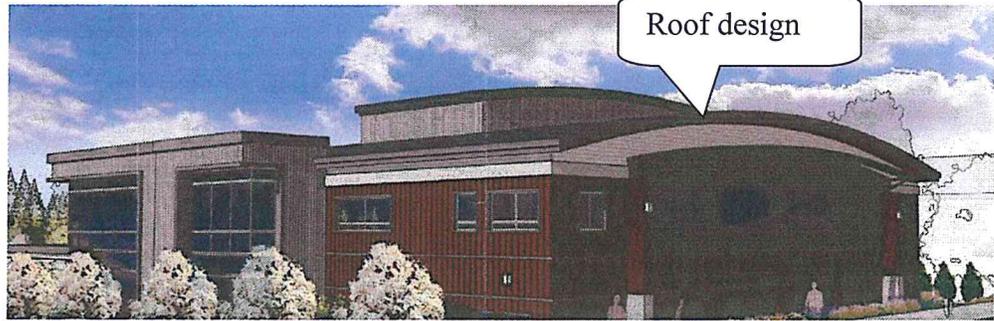
1. Waterfront Industrial: a) Single gable with low pitch; b) Repetitive gable with steep pitch; c) Shallow eaves; d) Small shed roof dormers; e) Monitor roof on ridge line; f) Flat panel skylights or roof window.
2. Commercial: a) Single gable with low pitch; b) Repetitive gable with steep pitch; c) Shallow eaves behind parapet wall; d) Flat or gable roof behind parapet wall; e) Structural skylights.
3. Residential: a) Steep gable with broad eaves; b) Steep hip with broad eaves; c) Dormers with gable, hip, or shed roofs; d) Flat panel skylights or roof window on secondary elevations; e) Turrets or large projecting window bays used sparingly.

Section 14.025 (I) identifies the roofing elements discouraged.

1. All Building Types: a) False mansard or other applied forms; b) Dome skylights.

Finding:

The structure will have low sloped barrel roof, and some



flat roof elements that incorporate a parapet wall. The roof lines are at multiple heights creating visual variation to the roof elements.

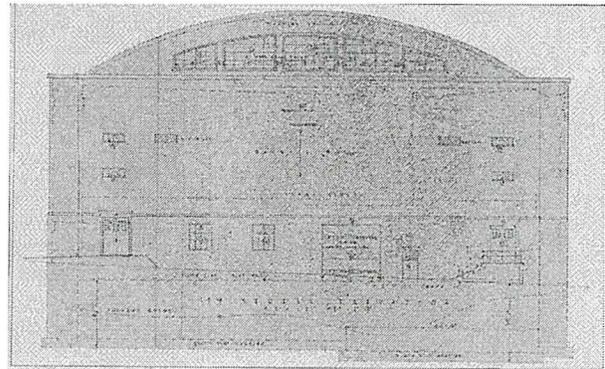
The multi-level barrel roofing are not common in Astoria, but can be found nearby in the Pavilion building, as well and the nearby Armory located at 1636 exchange

Curvilinear roofing nearby

This guideline is met.

- I. Section 14.025(J) identifies roofing materials encouraged.

1. All Building Types: a) Cedar shingle; b) Composition roofing; c) Roofing material in gray, brown, black, deep red, or other subdued colors.



2. Waterfront Industrial: a) Galvanized corrugated metal; b) Low profile standing seam, metal roof; c) Roll down.
3. Commercial: a) Built-up.

Section 14.025(K) identifies roofing materials discouraged.

1. All Building Types: a) High profile standing seam, metal roof; b) Brightly colored roofing material.

Finding: The roofing material proposed a mixture of steel and EPDM membrane with curved steel brackets. The roof system matches the roof type at the Pavilion building. The barrel vaulted over the main building, will be silver and gray. This guideline is met.

- J. Section 14.025(L) identifies signs encouraged.

1. All Building Types: a) Hanging blade signs; b) Signs painted on building facade; c) Signs applied to building facade; d) Front lit; e) Graphics historic in character.
2. Commercial: a) Exterior neon.

Section 14.025(M) identifies signs discouraged.

1. All Building Types: a) Pole mounted freestanding signs; b) Plastic or internal and back lit plastic.

Finding: The application indicates signage will include informational signs such as hours of operation, less than 1 square foot, and open/closed signs less than 1.5 square feet, which are exempt signs per code in Article 8.040. One monument sign and one building sign is proposed. Dimensions and materials shall be submitted with the sign permit pending an agreement by CMH and OHSU on a name for the building.

This guideline is met with the condition that the signage proposed, and any additional signage will need a sign permit and will be reviewed against the Sign Ordinance and compliance with this code section by the Planner.

K. Section 14.025(N) identifies exterior lighting encouraged.

1. All Building Types: a) Decorative lighting integrated with architecture; b) Metal halide or incandescent; c) Pedestrian and traffic signals combined with street lamps; d) Light fixtures that direct light downward and eliminate glare.
2. Waterfront Industrial: a) Industrial pan light with goose neck; b) Low bollard lighting.
3. Commercial: a) Historic street lamps along walks and parking lots.

Section 14.025(O) identifies exterior lighting discouraged.

1. All Building Types: a) Sodium vapor (amber); b) Fluorescent tube; c) Cobra head street lamps or other contemporary fixtures; d) Fixtures with undiffused, undirected light that do not focus the light to the ground and that will potentially destroy the night sky view.

Finding: Exterior parking lot lighting will match the parking lot lighting in the new lot completed in 2015. The proposed decorative lighting on the north elevation is down lighting from the metal eyebrow. Any additional lighting shall be downcast and not create glare into adjacent properties or rights-of-way. The cobra head parking lot style lights are discouraged for installation for building lighting, but provide consistency for parking lot lighting. This guideline is met.



Parking lot lighting from 2015 project

L. Section 14.025(P) identifies other design elements encouraged.

1. Commercial: a) Canvas awnings or fixed canopies for rain protection.

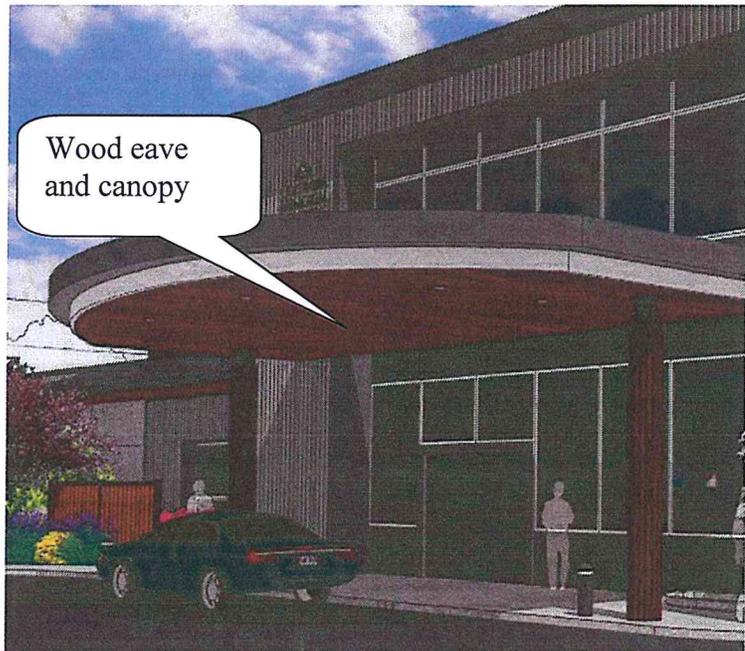
Section 14.025(Q) identifies other design elements discouraged.

2. Commercial: a) Vinyl awnings; b) Back lit awnings.

Finding:

The fixed wooden canopy for rain protection and ease of patient entry is encouraged in commercial buildings. The proposed project will have the large awning at the front entry for patient drop-off, and awning over the entry to the survivorship garden and west staff entry.

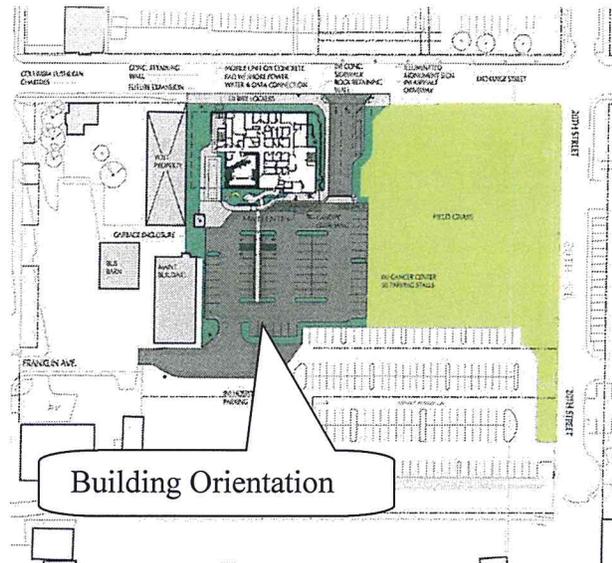
This guideline is met.



M. Section 14.030(A)(1) concerning building orientation states that development projects should form visually continuous, pedestrian-oriented street fronts with no vehicle use area between building faces and the street. Exceptions to this requirement may be allowed to form an outdoor space such as a plaza, courtyard, patio, or garden between a building and a sidewalk. Such a larger

front yard area should have landscaping, low walls, fencing, railings, a tree canopy, or other site improvements.

Finding: The proposed project covers multiple lots and a large area of what was previously John Warren football field. The building entrance is on the south side and provides access to the parking lot situated behind the building. The north façade fronts Exchange Street. The building is set close to Exchange Street positioned at the minimum 10' front yard setback. There is no vehicle use between the building face and Exchange Street.



The current design does not promote a pedestrian-oriented street front. The combination of a long front façade of glazed windows, and lack of main entrance does not provide an inviting area for pedestrians.

The applicant cites four key arguments in situating the entrance on the back of the building: 1. Noncompliance as acceptable when it is impractical for the location, 2. Existing buildings nearby that are not in compliance, 3. Building use specifically by clinic patients and 4. Design codes from other jurisdictions.

The design and orientation of the building should take more than just patient access into account. The neighboring areas are zoned for commercial, residential and family activities, all of which draw populations other than just patients to the area. The non-compliance of neighboring buildings is even more reason to provide a welcoming and pedestrian friendly northern façade for this area.

Exceptions are outlined in the code as follows: *“Exceptions to this requirement may be allowed to form an outdoor space such as a plaza, courtyard, patio, or garden between a building and a sidewalk. Such a larger front yard area should have landscaping, low walls, fencing, railings, a tree canopy, or other site improvements.”*

Portions of the design meet this criteria. Other site improvements shall be outlined by the applicant and submitted for approval in order to meet this criteria.

- N. Section 14.030(A)(2) concerning building orientation states that new uses should be sited to take advantage of the Columbia River and hillside views.

Finding: Orientation was heavily weighted to take advantage of views of the Columbia River. The applicant cites views of nature and the river are a key aspect to providing a healing environment for this patient population. The criteria is met.

- O. Section 14.030(A)(3) concerning building orientation states that if the proposed project is large or situated so as to become an entrance or major focus of the City, the design should recognize the project's prominence and should be both compatible with its surroundings and complementary to the City as a whole.

Finding: The proposed two-story project is large with a footprint of approximately 19,657 square feet. It will be situated on the former site of Warren field which was approximately 91,100 square feet. The entire structure will be visible from Marine Drive and Exchange Street. The orientation is compatible with the adjacent CMH building. The building is large, but not the first building visible upon entering the east end of town. Further expansion of the facilities on campus will also fall under Gateway Overlay and will require design review to maintain complementary design with the City and surrounding structures. This criteria is met.

- P. Section 14.030(B)(1) concerning building massing states that buildings should have a floor area ratio on their lots of at least 1:1 (One square foot of building area for one square foot of lot area), in order to maximize use of the land.

Finding: The project would have a floor area to lot ratio (FAR) of 0.22: The applicant has indicated that area of work encompasses an area of approximately 91,000 square feet, and there is no medical use that requires the entire 91,100 square foot area to be programmed to meet the 1:1 ratio. The large area provides sufficient space for the multiple buildings that make up the CMH campus.

This criteria is met, pending the applicant submits and receives approval for a variance request from the 1:1 FAR which will be reviewed and by the Community Development Department and submitted to the Astoria Planning Commission.

- Q. Section 14.030(B)(2) concerning building massing states that buildings should be a minimum of 24 feet in height from grade to highest point of the structure, excluding those features exempt from building height as identified in Development Code Section 3.075.

Finding: The proposed building height is 31' 3 and meets criteria. This exceeds the required minimum 24'. The AH-HC Zone has a maximum height of 35' above grade. The building height meets this approved criteria. The mechanical penthouse shown in Figure 2 of the application, is exempt from this height criteria per Article 3.075.

- R. Section 14.030(B)(3) concerning building massing states that the height, mass, and scale of buildings should be compatible with the site and adjacent buildings. Use of materials should promote harmony with surrounding historic structures and the character of the waterfront.

Finding The proposed structure is two-story. There are one, two, and three story commercial buildings in this area. Buildings along Exchange Street include the hospital and OSU Seafood Center which are large, multi-story buildings. One of the tallest commercial buildings in the overlay is the Fire Fighters Museum at 30th and Marine Drive, which is a flat roofed building at approximately 38' high. The remaining buildings are mostly one and two stories with a few three story residential structures. The materials will match buildings in the area and are compatible with those found on the existing CMH building. This criteria is met.

V. CONCLUSION AND RECOMMENDATION

In balance, the request meets the design objectives of the Design Review Guidelines. The applicant should be reminded of the following issues:

The applicant shall obtain all necessary City and building codes permits.

Staff recommends approval of the request with the following conditions.

1. A trash enclosure was proposed without designs submitted. A site plan for the enclosure shall be submitted to the Community Development Department. The final design for the solid waste disposal structure shall be reviewed and approved by the Planner at the time of the building permit application.
2. The applicant shall work with the solid waste disposal company, Recology, concerning the size and location of any refuse collection areas.
3. Any windows with grids shall be true divided or shall have external grids.
4. Any changes to the landscape plan shall be submitted for review and approval of the Planner at the time of the building permit application. Landscaping shall be installed prior to occupancy of the building.
5. The applicant shall work with the Fire Chief and City Engineering staff to establish striping to allow for emergency access. No on-street parking will be allowed near the entry on Exchange Street.
6. Any change in design or material or modifications to the proposed plans as described in this Staff Report shall be submitted to the Community Development Department for review.

7. The applicant shall submit a sign permit to the Community Development Department for the wall and monument signs, with detailed dimensions and materials proposed.
8. Further site improvements shall be outlined by the applicant to meet criteria for section 14.010 *“Exceptions to this requirement may be allowed to form an outdoor space such as a plaza, courtyard, patio, or garden between a building and a sidewalk. Such a larger front yard area should have landscaping, low walls, fencing, railings, a tree canopy...”* The applicant shall submit additional documentation for review outlining how they intend to meet this criteria.
9. The applicant shall submit a variance request from the 1:1 FAR which will be reviewed and by the Community Development Department and submitted to the Astoria Planning Commission.

APRIL 01, 2016



**COLUMBIA MEMORIAL HOSPITAL
CANCER COLLABORATIVE**

GENERAL DESIGN REVIEW SUBMISSION



Submitted To:
The City of Astoria
1095 Duane St
Astoria, Oregon 97103
Phone: (503) 325-5821



6969 SW Hampton Street | Portland, Oregon 97223

TABLE OF CONTENTS

Chapter		Page
	List of Figures	3
	City of Astoria Design Review Application	4
1	Project Team & Project Description	8
2	Article 2 - Zoning Applicable Approval Criteria	10
3	Article 3 - Additional Use and Development Standards Applicable Approval Criteria	14
4	Article 7 - Parking and Loading Applicable Approval Criteria	30
5	Article 8 - Signs Applicable Approval Criteria	62
6	Article 14 - Gateway Overlay Zone Applicable Approval Criteria	100
7	Figures	150
	 Appendix	
	 A - Kittelson & Associates, Inc. - Access Analysis	
	B - HLB/OTAK Professional Survey	
	C - Geotechnical Investigation and Site-Specific Seismic Hazard Evaluation	

LIST OF FIGURES (see Chapter 7)

FIGURE 1	Civil Site Plan
FIGURE 2	Building Elevations
FIGURE 3	Landscape Plan
FIGURE 4	Site Utility Plan
FIGURE 5	Building Perspectives
FIGURE 6	Architectural Site Plan
FIGURE 7	Door Details
FIGURE 8	Siding Details
FIGURE 9	Roofing Details
FIGURE 10	Window Details
FIGURE 11	Trim Details
FIGURE 12	Decorative Element Details
FIGURE 13	Current Conditions
FIGURE 14	County Tax Map



CITY OF ASTORIA
 Founded 1811 • Incorporated 1856
COMMUNITY DEVELOPMENT

DR _____

Fee: **\$250.00**

DESIGN REVIEW

Property Address: 1905 Exchange Street

Not applicable (See attached

Lot 14301 & 6400 Block 111 & 138 Subdivision Figure 14 County Tax Map

Map 80908DC & 80908DD Tax Lot 1, 2, 10, 11, 12 & 5, 6, 7, 8 Zone AH-HC

Applicant Name: PKA Architects (Josh Kolberg, Architect) on behalf of Columbia Memorial Hospital

Mailing Address: 6969 SW Hampton Street \ Portland, Oregon 97223

Phone 503.968.6800

Email: josh@pkaarchitects.com

Property Owner's Name: Columbia Memorial Hospital

Mailing Address: 2111 Exchange Street

Phone: 503.338.7505

Email: DGrey@columbiamemorial.org

Signature of Applicant: _____

Date: April 1, 2016

Signature of Property Owner _____

Date: April 1, 2016

Proposed Construction: Two story building dedicated to cancer related treatment and services for the Astoria, Clatsop County, and north Oregon coast communities.

Site Dimensions & Square Footage: TBD

Building Square Footage: 1st Floor: 10,479 2nd & 3rd Floor: 9,178 Garage: Not applicable

Accessory Building Information: No accessory buildings proposed

FILING INFORMATION: The Design Review Committee meets on the first Thursday of the month, as needed depending on date of applications. Complete applications must be received by the 23rd of each month. *A pre-application meeting with the Planner is required prior to the acceptance of the application as complete.* Only complete applications will be scheduled on the agenda. Your attendance at the Design Review Committee meeting is recommended.

For office use only:			
Application Complete:		Permit Info Into D-Base:	
Labels Prepared:		Tentative DRC Meeting Date:	
120 Days:			

All information concerning construction materials, design, dimensions, etc. is REQUIRED. If submitting large format plans, please also submit a reduced copy at 11" x 17" for reproducing.

Briefly address each of the Design Review Guidelines and state whether the project complies with the guideline, if applicable, and why this request should be approved. **Please provide manufacturer information and/or detailed information for use of any material or design not selected from the "Encouraged" list in the Design Guidelines.** (Use additional sheets if necessary.):

1. **Building Form.**

Basic Shape: The basic shape is rectangular, two stories, with a barrel vaulted roof.

Porches & Balustrade - Design, Dimension, Features, Materials: There are no porches proposed in this design.

Balconies & Balustrade - Design, Dimension, Features, Materials: There are no balconies proposed in this design.

Other: _____

2. **Windows.**

Material: Anodized aluminum storefront system. Possible curtainwall on north elevation.

Divided Windows (true divided, external muntins, etc): All window areas are true divided lites.

Operation (casement, single hung, etc.): Windows are not operable.

Size & Material of Exterior Casings (minimum 5/4" x 4"; provide detail diagram): Aluminum window sections are 2" wide x 4 1/2" deep.

Other: _____

3. **Exterior Wall Treatments.**

Material & Dimensions of Siding (note if material is smooth or textured): Vertical metal siding, Vertical board and batten fiber cement siding 3" battens - 12" boards. Horizontal fiber cement lap siding.

Decorative Features: _____

Other: Painted metal fascias and soffits. Wood under awning paneling. Exposed concrete at therapy vault.

4. **Doors.**

Material & Design: Entry door anodized aluminum automatic sliding door. South entry/exit door from classroom - anodized alum swing door. West exit door from corridor - anodized alum swing door.

Other: Other doors - painted metal doors.

5. **Roof Elements.**

Style and Pitch of Roof: Barrel vault over main building - low profile standing seam metal.

Material: Material: 1. Steel 2. EPDM membrane

Color: Color: 1. Silver grey 2. Black

Decorative Features (eave brackets, etc): Curved steel bracket over windows on second story of north elevation. Flat EPDM roof over entry lobby, entry canopy and west exit stair.

Other: _____

6. **Garage.**
 Garage Door Material & Design: There are no garages in this proposed design.
 Window Material & Design: _____
 Roof Style & Material: _____
 Other: _____

7. **Signs.**
 Dimension & Square footage: 32 square foot monument sign. One building sign.
 Location: Building sign on south facade above entry.
 Type, Material & Design: Same material as Pavilion building (not lit).
 Other: _____

8. **Exterior Lighting.**
 Fixture & Lamp Design: Downlight LED fixtures.
 Location: Parking lot lighting, canopy downlights, building walls, north facade, at doors.
 Other: _____

9. **Other Design Elements.**
 (Fences, out buildings, corner boards, belt course, etc. with dimensions): Decorative fence at garden on south side of building. Painted metal frame and natural vertical wood slats. Wood clad columns at south entry canopy and at north facade.

10. **Building Orientation.**
The building will be oriented along Exchange Street between 18th and 20th just east of the Yost Property.
The entry drive will be along Exchange with accessible building entry opposite patient parking.

11. **Building Massing.**
 Building to Lot Ratio: The site as defined generally by the scope of work on Warren Field (91,000 sf)
 Other: _____ and the building gross square footage (19,657 sf) - 0.22 FAR

12. **Access and Parking Design.**
 Number of Off-street Spaces: 58 total parking spaces (including 4 accessible spaces)
 Other: _____

13. **Landscaping.**
There is a detailed landscaping plan that can be viewed in Figure 3. All surrounds of this development and parking will be professionally landscaped.

14. **Underground Utilities.**
Underground utilities consist of water, sanitary sewer, storm drainage, electrical/telecommunications conduit and natural gas. The building is served by 2" domestic and 4" fire protection water services from the relocated 12" public main in Exchange Street. A new sanitary sewer lateral will connect to the existing 10" sewer lateral.

PLANS: A site plan indicating location of the proposed structure on the property is required. Diagrams showing the proposed construction indicating style and type of materials proposed to be used are required. Scaled free-hand drawings are acceptable. The City may be able to provide some technical assistance on your proposal if it is adjacent to a historic structure and will require additional review by the Historic Landmarks Commission.

1 PROJECT TEAM & PROJECT DESCRIPTION

Owner **Columbia Memorial Hospital**
Erik Thorsen, Chief Executive Officer
Trece Gurrad, Vice President of Patient Care Services
Chris Laman, Director of Pharmacy & Cancer Center Services

Owner's Representatives

Al Jaques, CM Services Co.
Ted Jacobsen, Sr. Project Manager, Day CPM

Architect **Petersen Kolberg & Associates**
Steve Kolberg, AIA, Principal
Josh Kolberg, Architect, Project Manager

Civil **KPFF**
Adam Roth, Senior Project Manager

Traffic Consultant **Kittelson & Associates, Inc.**
Julia Kuhn, Principal Engineer

Landscape Architect **Macdonald Environmental Planning, PC**
Laurel Macdonald, Principal

PROJECT DESCRIPTION

Over the past 8 years oncology services at Columbia Memorial Hospital (CMH) have gone from a room in the same day surgery department where two patients would sit receiving chemotherapy, to a full medical oncology clinic with an infusion center. As we have grown, so has our desire to provide more oncology services here in Astoria. Currently, 75-100 patients per year in our area travel to Portland, Longview, or Seattle for radiation therapy. This results in a great deal of burden on the patient and their Care Partners. Providing radiation therapy in our community will prevent the need for these long drives for care. The building will provide space for a second medical oncologist. It will also enable us to offer supportive care programs in the same location where patients see their physicians. In addition to providing patient navigation, nutrition services, support groups, counseling, library resources, education, social work services, financial and logistical support, patients will also have access to alternative therapies such as yoga, acupuncture, exercise classes and even make overs

The new cancer center will be a 19,000 + square-foot, 2-story building located at the west end of our campus. Our goal is to make the new building an optimal healing environment. It will have a peaceful and comfortable atmosphere with many windows to allow natural lighting. Front line staff, current and former patients, and Planetree A-Teams will all have input on the design. The official ground breaking will be held on August 4th, 2016 with the goal to open in September of 2017.

PURPOSE OF PROJECT

Knight Cancer has a mission to make Oregon's death rate from cancer the lowest in the U.S. In order to accomplish this goal they are developing partnerships like this one to provide evidence based, state of the art care in rural communities. The partnership with our hospital will be the first of its kind in the state for Knight Cancer and will raise the level of cancer care in this rural area to levels once thought unattainable.

PRIMARY TYPE OF PATIENTS SERVED

The cancer center serves as a place for monitoring and treatment of adult patients with cancer related diagnoses. We treat patients on an outpatient basis so that they can focus on getting better in the comfort of their own homes and maintain as normal a lifestyle that their treatment will allow.

CONDITIONS OF TYPICAL PATIENT

Based on registry data from 2014 and 2015 our top sites were as follows: Leukemia / Lymphoma, Lung Cancer, Breast Cancer, Prostate Cancer, Bladder Cancer, and Colorectal Cancer

SERVICES PROVIDED

The center will offer radiation and medical oncology physician services, radiation physicist, dosimetrist, skilled biotherapy nursing, pharmacotherapy, imaging, laboratory, social services, financial counseling, dietician services, a resource library as well as integrative therapies such as massage and acupuncture and a space for support groups and exercise classes like yoga.

STAFFING

The cancer center will be staffed with a diverse group of experts in multiple specialties. Below is a list of these specialist and a description of the services they will provide:

- Radiation Oncologist: a specialist physician who uses ionizing radiation (such as mega voltage X-rays or radionuclides) in the treatment of cancer.
- Medical Oncologist: a physician who specializes in diagnosing and treating cancer using chemotherapy, hormonal therapy, biological therapy, and targeted therapy.
- Radiation physicists: a physicist who works in the medical field of radiology, specializing in diagnostics, intervention, nuclear medicine and radio therapy.
- Dosimetrist: a member of the radiation oncology team who has knowledge of the overall characteristics and clinical relevance of radiation oncology treatment machines and equipment, is cognizant of procedures commonly used in brachytherapy and has the education and expertise necessary to generate radiation.
- Biotherapy Nurses: nurses who have knowledge of cancer basics and are in a position to regularly administer chemotherapy and biotherapy.
- Oncology Pharmacist: a specialist actively engaged in all aspects of cancer care, from chemotherapy dose preparation and safety checks, to educating patients and staff about side effects from chemotherapy.
- Nurse Navigator: a professional registered nurse with oncology-specific clinical knowledge who offers individualized assistance to patients, families, and caregivers to help overcome healthcare system barriers.

2 ARTICLE 2 - ZONING APPLICABLE APPROVAL CRITERIA

2.918. PURPOSE.

The purpose of the Attached Housing/Health Care Zone is to develop the area as an attached and senior housing area at a minimum density of 13 units per acre, with medical uses allowed with appropriate buffers. It is intended to be a residential neighborhood in scale and character.

RESPONSE: The use of the proposed development is for medical office space dedicated to the treatment of cancer patients in Astoria, Clatsop County, and the northern Oregon Coast. The site plan (Figure 1) shows code required setbacks to maintain appropriate buffers.

2.920. USES PERMITTED OUTRIGHT.

The following uses and their accessory uses are permitted in an AH-HC Zone if the Community Development Director determines that the uses will not violate standards referred to in Sections 2.924 through 2.934, additional Development Code provisions, the Comprehensive Plan, and other City laws:

1. Congregate care facility.
2. Family day care center.
3. Heliport associated with a hospital.
4. Home occupation which satisfies the requirements in Section 3.095.
5. Nursing home.
6. Medical or health care service establishments.
7. Multi-family dwelling.
8. Residential facility.
9. Residential home.
10. Single-family dwelling, located above or below the first floor, with permitted commercial facilities on the first floor of the structure.
11. Two-family dwelling.
12. Transportation facilities.

RESPONSE: The proposed development is a medical/health care service.

2.922. CONDITIONAL USES PERMITTED.

The following uses and their accessory uses are permitted in a AH-HC Zone if the Planning Commission, after a public hearing, determines that the location and development plans comply with applicable standards referred to in Sections 2.924 through 2.934, additional Development Code provisions, the Comprehensive Plan, and other City laws:

1. Arts and crafts studio.
2. Business service establishment.
3. Day care center.
4. Educational service establishment.
5. Personal service establishment.

6. *Public or semi-public use.*
7. *Retail sales establishment not exceeding 3,000 square feet gross floor area.*
8. *Temporary use meeting the requirements of Section 3.240.*

RESPONSE: The applicant is not proposing a conditional use development.

2.924. YARDS.

The minimum yard requirements in an AH-HC Zone will be as follows:

1. *The minimum front yard will be 10 feet. However, the minimum front yard for porches, bay windows, and stairways will be 6 feet.*
2. *The minimum rear yard will be 5 feet.*

RESPONSE: Please see Figure 1 which shows the proposed development site plan. The building setback in front along Exchange Street is 10 feet from the back of the public sidewalk. The rear yard setback is in excess of 5 feet.

2.926. DENSITY.

Residential uses will have a minimum density of 13 units per acre.

RESPONSE: The proposed development is not residential.

2.928. LOT COVERAGE.

Buildings will not cover more than 80% of the lot area.

RESPONSE: Per Figure 1, the proposed development covers only approximately 11.5% of the total site area as generally defined by the scope of work.

2.930. LANDSCAPED OPEN AREA.

A minimum of 20% of the total lot area will be maintained as a landscaped open area.

RESPONSE: Per Figure 1, the proposed development has landscaped open area cover of approximately 25% of the total area.

2.932. HEIGHT OF STRUCTURES.

No structure will exceed a height of 35 feet above grade.

RESPONSE: Please see Figure 2. Figure 2 identifies the height of the building as measured per Section 1.400. DEFINITIONS: "Height, Building". The building height is measured at 31'-9" which meets this approval criteria. The mechanical penthouse as shown in Figure 2 is exempt from this criteria per Section 3.075. EXCEPTION TO BUILDING HEIGHT LIMITATIONS(A)(2) which lists mechanical penthouses as exempt features from the building height limit.

2.934. OTHER APPLICABLE USE STANDARDS.

1. *All uses except single-family and two family dwellings shall meet the landscaping requirements of Sections 3.105 through 3.120.*

RESPONSE: See responses for Article 3 in Chapter 3.

2. *Outdoor storage areas will be enclosed by appropriate vegetation, fencing, or walls. This requirement does not apply to outdoor retail sales areas.*

RESPONSE: The outdoor trash enclosure will be enclosed by walls and landscaping.

3. *Where feasible, joint access points and parking facilities for more than one use should be established. This standard does not apply to multi-family residential developments.*

RESPONSE: The vehicular access point off of Exchange Street is part of a future master plan for development of John Warren field. This access point will eventually be a main access point for a future replacement hospital built to the east of the proposed cancer center.

4. *Access drives and parking areas should, where possible, be located on side streets or non-arterial streets in order to minimize congestion on Marine Drive.*

RESPONSE: The access drive for this development is off of Exchange Street.

5. *All uses will comply with access, parking, and loading standards in Article 7.*

RESPONSE: Please see Chapter 4 for responses to Article 7 approval criteria.

6. *Conditional uses will meet the requirements in Article 11.*

RESPONSE: The proposed development is not a conditional use.

7. *Signs will comply with requirements in Article 8 and specifically, the Specific regulations of the C-3 Zone in Section 8.150.*

RESPONSE: Please see Chapter 5 for responses to Article 8 approval criteria.

8. *All structures will have storm drainage facilities that are channeled into the public storm drainage system or a natural drainage system approved by the City Engineer. Developments affecting natural drainage shall be approved by the City Engineer.*

RESPONSE: Please see the site utility plan in Figure 4. All storm drainage facilities will channel to the public storm drainage system.

9. *Where new development is within 100 feet of a known landslide hazard, a site investigation report will be prepared by a registered geologist. Recommendations contained in the site report will be incorporated into the building plans.*

RESPONSE: New development is not within 100 feet of a known landslide hazard.

10. *For uses located within the Astor-East Urban Renewal District, refer to the Urban Renewal Plan for additional standards.*

RESPONSE: This is not an applicable standard for this development.

11. *All uses will comply with the requirements of the Gateway Overlay Zone in Sections 14.005 to 14.030.*

RESPONSE: Please see Chapter 6 for responses to Article 14 approval criteria.

12. *A buffer of 100' shall be maintained between residential uses and nonresidential uses within the boundaries of the AH-HC Zone. The buffer area shall not include structures, but may include parking and landscaped open space.*

RESPONSE: No residential uses are within 100 feet of the proposed cancer center development.

13. *The western 100' of land along the 20th Street right-of-way shall be reserved for medical and health care facilities.*

RESPONSE: This standard is met with the cancer center development.

14. *When a commercial use in an AH-HC Zone abuts a lot in a residential zone there will be an attractively designed and maintained buffer of at least five (5) feet in width, which can be in the form of hedges, fencing, or walls.*

RESPONSE: No part of the cancer center development abuts a lot in a residential zone.

15. *Outdoor lighting in the residential areas shall be designed and placed so as not to cast glare into adjacent residential properties. The Community Development Director may require the shielding or removal of such lighting where it is determined that existing lighting is adversely affecting adjacent residences.*

RESPONSE: No part of the cancer center development abuts a lot in a residential zone.

3 ARTICLE 3 - ADDITIONAL USE AND DEVELOPMENT STANDARDS APPLICABLE APPROVAL CRITERIA

3.005. ACCESS TO STREETS.

Every lot shall abut a street, other than an alley, for at least 25 feet.

RESPONSE: The lots in this proposed development abut Exchange Street in excess of 25 feet.

3.008. VEHICULAR ACCESS AND CIRCULATION.

A. Purpose and Intent.

Section 3.008 implements the street access policies of the City of Astoria Transportation System Plan. It is intended to promote safe vehicle access and egress to properties, while maintaining traffic operations in conformance with adopted standards. "Safety," for the purposes of this Section, extends to all modes of transportation.

RESPONSE: This section is not an approval standard.

B. Applicability.

Section 3.008 applies to new development and changes in land use involving a new or modified street connection. Except where the standards of a roadway authority other than the City supersede City standards, this section applies to all connections to a street, and to driveways and walkways. The Community Development Director may grant adjustments of 10% or less of the quantitative standard pursuant to Class 1 variance procedures in Article 12. The Planning Commission may grant adjustments of more than 10% of the standard pursuant to Class 2 variance procedures in Article 12. For transportation facility improvement requirements, refer to Section 3.015.

RESPONSE: This project is a new development so section 3.008 applies but 3.008(B) is not an approval standard.

C. Traffic Study Requirements.

The City in reviewing a development proposal or an action requiring an approach permit may require a Traffic Impact Study, pursuant to Subsection 3.015.A.5, to determine compliance with this code.

RESPONSE: A traffic study was conducted for this development and can be found in its entirety in Appendix A.

D. Approach and Driveway Development Standards.

Approaches and driveways taking access on City streets shall conform to all of the following development standards:

1. The number of approaches on higher classification streets (e.g., collector and arterial streets) shall be minimized; where practicable, access shall be taken first from a lower classification street. Access to single family residential development should not be provided to an arterial or collector roadway.

RESPONSE: Only one drive access is proposed on Exchange Street.

2. Approaches shall conform to the spacing standards of Subsections E and F below, and shall conform to minimum sight distance and channelization standards of the roadway authority.

RESPONSE: The approach on Exchange shall comply with the referenced code sections.

3. Driveways shall be paved and meet applicable construction standards in the Astoria Engineering Design Standards (Chapter 4 - Roadways).

RESPONSE: The drive access on Exchange shall comply with the referenced code sections.

4. The City Engineer may limit the number or location of connections to a street, or limit directional travel at an approach to one-way, right-turn only, or other restrictions, where the roadway authority requires mitigation to alleviate safety or traffic operations concerns.

RESPONSE: This section is not an approval standard.

5. Where the spacing standards of the roadway authority limit the number or location of connections to a street or highway, the Community Development Director, Planning Commission, or City Engineer as applicable may require a driveway extend to one or more edges of a parcel and be designed to allow for future extension and inter-parcel circulation as adjacent properties develop. The Community Development Director, Planning Commission, or City Engineer as applicable may also require the owner(s) of the subject site to record an access easement for future joint use of the approach and driveway as the adjacent properties develop. Where a land division abuts or contains an existing or proposed arterial street, the Planning Commission may require marginal access streets, reverse frontage lots with suitable depth, screen planting contained in a non-access reservation along the rear or side property line, or other treatment necessary for adequate protection of residential properties and to afford separation of through and local traffic.

RESPONSE: The traffic study found in Appendix A confirms that the proposed drive access on Exchange has a limited impact on that street and adjacent intersection. The access point on Exchange will eventually be utilized by Columbia Memorial Hospital as part of a large campus/hospital replacement project in the future.

6. Where applicable codes require emergency vehicle access, approaches and driveways shall be designed and constructed to accommodate emergency vehicle apparatus and shall conform to applicable fire protection requirements. The City Engineer may restrict parking, require signage, or require other public safety improvements pursuant to the recommendations of an emergency service provider.

RESPONSE: Ambulances will have access to this site. The Fire Chief commented in the pre-application that the building frontage on Exchange Street (the south side of Exchange) be red striped to prevent on-street parking to allow for fire engine access. The drive connection off of Exchange will also be painted

red for fire engine access.

7. As applicable, approaches and driveways shall be designed and constructed to accommodate truck/trailer-turning movements.

RESPONSE: Truck and trailer approaches are possible with the proposed Exchange Street drive access.

8. Residential driveways shall have a minimum depth of 16 feet between the property line and any structure to accommodate on-site vehicular parking.

RESPONSE: The proposed development is not residential.

9. Driveways shall be designed so that vehicle areas, including but not limited to drive-up and drive-through facilities and vehicle storage and service areas, do not obstruct any public right-of-way and do not result in vehicles stacking or backing up onto a street.

RESPONSE: The proposed drive access has a large queueing potential for cars entering the parking lot such that given the trip generation there will be no stacking or backing up onto Exchange.

10. Approaches and driveways shall not be wider than necessary to safely accommodate projected peak hour trips and turning movements, and shall be designed to minimize crossing distances for pedestrians.

RESPONSE: Peak hour exiting will be appropriately accommodated by the left and right hand turn lane and this curb cut design is minimized and striped appropriately for pedestrian crossing.

11. As it deems necessary for pedestrian safety, the City Engineer, in consultation with the roadway authority, as applicable, may require traffic-calming features (such as speed cushions and tables, textured driveway surfaces, curb extensions, signage or traffic control devices, or other features) be installed on or in the vicinity of a site as a condition of development approval.

RESPONSE: This section is not an approval standard.

12. Construction of approaches along acceleration or deceleration lanes, and along tapered (reduced width) portions of a roadway, shall be avoided; except where no reasonable alternative exists and the approach does not create safety or traffic operations concern.

RESPONSE: This section is not an applicable approval standard for this development.

13. Approaches and driveways shall be located and designed to allow for safe maneuvering in and around loading areas, while avoiding conflicts with pedestrians, parking, landscaping, and buildings.

RESPONSE: A specialty concrete pad has been designed on the west side of the building which will be used for deliveries of materials and is out of the way of pedestrian and patient access. This pad will

also be the parking location for a mobile PET/CT machine which will be on the site once per week for speciality imaging services for the community.

14. Where an accessible route is required pursuant to the Americans with Disabilities Act, approaches and driveways shall meet accessibility requirements.

RESPONSE: The sidewalk connection from the front entry of the building to the public sidewalk on Exchange meets accessibility guidelines.

15. The City Engineer may require changes to the proposed configuration and design of an approach, including the number of drive aisles or lanes, surfacing, traffic-calming features, allowable turning movements, and other changes or mitigation, to ensure traffic safety and operations.

RESPONSE: This section is not an approval standard.

16. Where a new approach onto a State highway or a change of use adjacent to a State highway requires ODOT approval, the applicant is responsible for obtaining ODOT approval. The Community Development Director or Planning Commission, as applicable, may approve a development conditionally, requiring the applicant first obtain required ODOT permit(s) before commencing development, in which case ODOT will work cooperatively with the applicant and the City to avoid unnecessary delays.

RESPONSE: This section is not an applicable approval standard for this development.

17. Where an approach or driveway crosses a drainage ditch, canal, railroad, or other feature that is under the jurisdiction of another agency, the applicant is responsible for obtaining all required approvals and permits from that agency prior to commencing development.

RESPONSE: This section is not an applicable approval standard for this development.

18. Where a proposed driveway crosses a culvert or drainage ditch, the City Engineer may require the developer to install a culvert extending under and beyond the edges of the driveway on both sides of it, pursuant to applicable Astoria Engineering Design Standards.

RESPONSE: This section is not an applicable approval standard for this development.

19. Except as otherwise required by the applicable roadway authority or waived by the City Engineer, temporary driveways providing access to a construction site or staging area shall be paved or graveled to prevent tracking of mud onto adjacent paved streets.

RESPONSE: Per this approval standard, temporary access during construction will be graveled.

E. Approach Separation from Street Intersections.

Except as provided by Section 3.008.G, approach, driveway, and intersection spacing shall comply with the minimum distance standards provided in Table 1 (Spacing Standards) of the Astoria Transportation System Plan.

RESPONSE: See response to 3.008(G) below.

F. Vision Clearance.

Refer to Section 6.100 (Vision Clearance Area) of the City code.

RESPONSE: Per section 6.100 of the City code, the proposed development will adhere to the vision clearance area as depicted in Figure 4 of that section. Within this clearance area, there will be no planting or signage between 30 inches and 8 feet in height.

G. Exceptions and Adjustments to Approach Separation from Street Intersections.

The City decision body may approve adjustments to the spacing standards of Subsection E above, where an existing connection to a City street does not meet the standards of the roadway authority and the proposed development moves in the direction of code compliance. The City Engineer may also approve a deviation to the spacing standards on City streets where it finds that mitigation measures, such as consolidated access (removal of one access), joint use driveways (more than one property uses same access), directional limitations (e.g., one-way), turning restrictions (e.g., right in/out only), or other mitigation alleviate traffic operations and safety concerns, through an administrative review procedure with notice pursuant to Section 9.020.

RESPONSE: This section is not an applicable approval standard for this development.

H. Joint Use Access Easement and Maintenance Agreement.

Where the City approves a joint use driveway, the property owners shall record an easement with the deed allowing joint use or cross access between adjacent properties. The owners of the properties agreeing to joint use of the driveway shall record a joint maintenance agreement with the deed defining maintenance responsibilities of property owners. The applicant shall provide a fully executed copy of the agreement to the City for its records, but the City is not responsible for maintaining the driveway or resolving any dispute between property owners. This easement/agreement requirement shall also apply to separate properties under the same ownership.

RESPONSE: This section is not an applicable approval standard for this development.

3.010. ON-SITE PEDESTRIAN AND BICYCLE ACCESS AND CIRCULATION.

A. Purpose and Intent.

Section 3.010 implements the pedestrian and bicycle access and connectivity policies of City of Astoria Transportation System Plan. It is intended to provide for safe, reasonably direct, and convenient pedestrian and bicycle access and circulation.

RESPONSE: This section is not an approval standard.

B. Applicability.

Section 3.010 applies to new development and changes in land use involving a new or modified street connection. Except where the standards of a roadway authority other than the City supersede City standards, this section applies to all connections to a street, and to driveways and walkways. The Community Development Director may grant adjustments of 10% or less of the quantitative standard pursuant to Class 1 variance procedures in Article 12. The Planning Commission may grant adjustments of more than 10% of the standard pursuant to Class 2 variance procedures in Article 12. For transportation facility improvement requirements, refer to Section 3.015.

RESPONSE: Section 3.010 will apply to this development.

C. Standards.

Applicable development shall conform to all of the following standards for pedestrian access and circulation:

1. Continuous Walkway System. A walkway system shall extend throughout the development site and connect to adjacent sidewalks, if any, and to all future phases of the development, as applicable.

RESPONSE: Per the site plan in Figure 1, there is a continuous walkway system that connects from the public street on Exchange, to the building entry, to the survivorship garden, and to the mobile PET/CT and staff entry point. This system also connects to the ADA parking stalls and the parking lot is on grade with entry of the building. Future development of the property to the east of this site will modify this plan and connect to these points.

2. Safe, Direct, and Convenient Walkways. Walkways within developments shall provide safe, reasonably direct, and convenient connections between primary building entrances and all adjacent parking areas, open spaces, recreational areas/playgrounds, and public rights-of-way based on all of the following criteria:
 - a. The walkway is designed primarily for pedestrian and bicycle safety and convenience, meaning it is reasonably free from hazards and obstructions, and provides a reasonably smooth and consistent surface and direct route of travel between destinations. The Community Development Director or Planning Commission as applicable may require landscape buffering between access ways and adjacent parking lots or driveways to mitigate safety concerns.

RESPONSE: All sidewalk areas are on flat grade from the building entry, staff entry, ADA parking and standard parking lot. The sidewalk connection to Exchange Street is at an ADA accessible slope.

- b. The walkway is reasonably direct. A walkway is reasonably direct when it follows a route that does not deviate unnecessarily from a straight line or it does not involve a significant amount of out-of-direction travel.

RESPONSE: No out of direction travel is proposed for the sidewalk connections.

- c. The walkway network connects to primary building entrances and,

where required, meets Americans With Disabilities Act requirements.

RESPONSE: All proposed building entrances are connected via the sidewalk system and all entries are ADA accessible.

3. Vehicle/Walkway Separation. Except as required for parking area and driveway crossings, per Subsection 4 below, where a walkway abuts a driveway it shall be raised six (6) inches and curbed along the edge of the driveway/street. Alternatively, the Community Development Director or Planning Commission may approve a walkway abutting a driveway at the same grade as the driveway if the walkway is physically separated from all vehicle-maneuvering areas; for example, a row of bollards designed for use in parking areas, with adequate minimum spacing between them to prevent vehicles from entering the walkway.

RESPONSE: All sidewalks have a 6" curb with the exception of the driveway crossing and for patient ease of access at the building entry. Bollards will be used to prevent vehicle access.

4. Parking Area and Driveway Crossings. Where a walkway crosses a parking area or driveway, it shall be clearly marked with contrasting paving materials (e.g., pavers, light-color concrete inlay between asphalt, or similar contrast). The crossing may be part of a speed table to improve driver-visibility of pedestrians. Painted or thermoplastic striping and other types of non-permanent applications are discouraged, but may be approved for lesser-used crossings not exceeding 24 feet in length.

RESPONSE: The proposed development shall adhere to this standard. The ADA crossing path will be concrete (color differential) and thermoplastic striping.

5. Walkway Width and Surface. Walkways shall be constructed of concrete, asphalt, brick/masonry pavers, or another durable surface, as approved by the City Engineer and meeting Americans With Disabilities Act requirements, with a surface not less than six (6) feet wide. The Community Development Director or Planning Commission as applicable may require a wider walkway where pedestrian traffic warrants.

RESPONSE: All walkway widths are a minimum of 5 feet wide.

6. Mid-Block Walkways. Walkways through blocks for pedestrian and bicycle access shall be provided at least every 330 feet for blocks that exceed the spacing standards in Table 1 of the Transportation System Plan. Road crossings shall be similarly provided and these are addressed in the Transportation System Plan and the Astoria Engineering Design Standards for Roadways (Chapter 4).

RESPONSE: This section is not an applicable approval standard for this development.

7. Shared-Use Pathways. Shared-use pathways, designed for use by

bicyclists, pedestrians, and other non-motorized users, shall conform to the transportation standards of Section 3.015, and Figure 18 in the Astoria Transportation System Plan. Where approved, shared-use pathways shall be constructed of asphalt, concrete, or another durable surface, as approved by the City Engineer and meeting Americans With Disabilities Act requirements. The City may reduce the width of the paved shared-use path to a minimum of eight (8) feet in constrained areas located in steep, environmentally sensitive, rural, historic, or development- limited areas of the City."

RESPONSE: This section is not an applicable approval standard for this development.

3.015. TRANSPORTATION STANDARDS.

A. General Requirements.

1. Except as provided by waiver, deferral, and variance provisions in Subsection 7 below, and the adopted Astoria Engineering Design Standards for In-fill Development, existing substandard streets and planned streets within or abutting a proposed development shall be improved pursuant to the standards of this Section as a condition of development approval. Proposed development shall include proposed land divisions, new buildings or structures that require building permits, or substantial changes to a site, use, or structure. For the purposes of this section, "substantial change" is defined as development activity that requires a building permit and involves one or more of the following:
 - a. A new dwelling unit.
 - b. An increase in gross floor area of [50%] or more.
 - c. A projected increase in vehicle trips [as determined by the City Engineer].

RESPONSE: The proposed development is a new building.

2. All street improvements, including the extension or widening of existing streets and public access ways, shall conform to this Section, and shall be constructed consistent with the Astoria Engineering Design Standards.

RESPONSE: This section is not an approval standard.

3. All new streets should be contained within a public right-of-way; pedestrian and shared-use access ways may be contained within a right-of-way or a public access easement, subject to review and approval of the Community Development Director or Planning Commission (for land divisions) or the City Engineer (for existing plats). The design and improvement of any street in a land division shall be subject to all requirements prescribed by this ordinance for public streets. The land divider shall provide for the permanent maintenance of any street required for access to property in a private street subdivision or a major partition.

RESPONSE: This section is not an applicable approval standard for this development.

4. Rough Proportionality. The rough proportionality requirements of this section apply to both frontage and non-frontage improvements. A proportionality analysis will be conducted by the City Engineer for any proposed development that triggers transportation facility improvements pursuant to Section 3.015. The City Engineer will take into consideration any benefits that are estimated to accrue to the development property as a result of any required transportation facility improvements. A proportionality determination can be appealed pursuant to Section 9.040. The following general provisions apply whenever a proportionality analysis is conducted.
 - a. Mitigation of impacts due to increased demand for transportation facilities associated with the proposed development shall be provided in rough proportion to the transportation impacts of the proposed development. When applicable, anticipated impacts will be determined by the Traffic Impact Study (TIS) pursuant to Section 3.015.A.5. When no TIS is required, anticipated impacts will be determined by the City Engineer.

RESPONSE: We look forward to discussing the City Engineers analysis.

- b. The following shall be considered when determining proportional improvements
 1. Condition and capacity of existing facilities within the impact area in relation to City standards. The impact area is generally defined as the area within a one-half (1/2) mile radius of the proposed development. If a Traffic Impact Study is required pursuant to Section 3.015.A.5, the impact area is the TIS study area.

RESPONSE: This section is not an approval standard.

2. Existing vehicle, bicycle, pedestrian, and transit use within the impact area.

RESPONSE: This section is not an approval standard.

3. The effect of increased demand on transportation facilities and other approved, but not yet constructed, development projects within the impact area that is associated with the proposed development.

RESPONSE: This section is not an approval standard. The traffic study in Appendix A can be used as a reference for this analysis.

4. Applicable Transportation System Plan goals, policies, and plans.

RESPONSE: This section is not an approval standard.

5. Whether any route affected by increased transportation demand within the impact area is listed in any City program including school trip safety, neighborhood traffic management, capital improvement, system development improvement, or others.

RESPONSE: This section is not an approval standard.

6. Accident history within the impact area.

RESPONSE: This section is not an approval standard.

7. Potential increased safety risks to transportation facility users, including pedestrians and cyclists.

RESPONSE: This section is not an approval standard.

8. Potential benefit the development property will receive as a result of the construction of any required transportation facility improvements.

RESPONSE: This section is not an approval standard.

9. Other considerations as may be identified in the review process.

RESPONSE: This section is not an approval standard.

5. Traffic Impact Studies. In order to comply with and implement the State Transportation Planning Rule, the City shall adopt a process to coordinate the review of land use applications with roadway authorities and apply conditions to development proposals in order to minimize impacts and protect transportation facilities, which can be done by establishing requirements for Traffic Impact Studies.
 - a. When a Traffic Impact Study is Required. The City or other road authority with jurisdiction may require a Traffic Impact Study (TIS) as part of an application for development, a change in use, or a change in access. Based on information provided by the applicant about the proposed development, the City will determine when a TIS is required and will consider the following when making that determination.

RESPONSE: A traffic study was conducted for this development and can be found in its entirety in Appendix A.

6. The City may attach conditions of approval to land use decisions as needed to satisfy the transportation facility requirements of Section 3.015 and to mitigate transportation impacts identified in the Traffic Impact Study.

RESPONSE: This section is not an approval standard.

7. Variances to standards in Section 3.015.A of 10% or less may be granted by administrative review. Variances of more than 10% of the standards in this Section may be granted by the Community Development Director pursuant to Class 1 variance procedures in Article 12. The Community Development Director or Planning Commission, as applicable, may waive or defer improvements for land divisions and on-site improvements, and the City Engineer may waive or defer improvements for existing plats and off-site improvements. When the Community Development Director, Planning Commission, or City Engineer agrees to defer a street improvement, they shall do so only where the property owner agrees not to remonstrate against the formation of a local improvement district in the future. Standard street improvements, including sidewalk, roadway, bicycle lane, undergrounding of utilities, and/or landscaping, as applicable, may be waived or deferral may be allowed where one or more of the following conditions are met:
 - a. The standard improvement conflicts with an adopted capital improvement plan;

RESPONSE: This section is not an approval standard.

- b. The standard improvement would create a safety hazard;

RESPONSE: This section is not an approval standard.

- c. It is unlikely due to the developed condition of adjacent property that the subject improvement would be extended in the foreseeable future, and the improvement under consideration does not by itself significantly improve transportation operations or safety;

RESPONSE: This section is not an approval standard.

- d. The improvement under consideration is part of an approved partition in a residential zone and the proposed partition does not create any new street.

RESPONSE: This section is not an approval standard.

B. Street Location, Alignment, Extension, Grades, and Names.

1. All new streets, to the extent practicable, shall connect to the existing street network and allow for the continuation of an interconnected street network, consistent with adopted public facility plans, including the Astoria Transportation System Plan (Figures 10-12), and pursuant to Subsection 3.015.D, Transportation Connectivity and Future Street Plans.

RESPONSE: No new streets are proposed in this development.

2. Specific street locations and alignments shall be determined in relation to existing

and planned streets, topographic conditions, public convenience and safety, and in appropriate relation to the proposed use of the land to be served by such streets. As far as is practical, streets shall be in alignment with existing streets by continuations of the center lines thereof. Staggered street alignment resulting in "T" intersections shall, wherever practical, leave a minimum distance of 200 feet between the center lines of streets having approximately the same direction, and in no case, shall be less than 150 feet.

RESPONSE: No new streets are proposed in this development. The Exchange drive access point for parking is in excess of 200 feet from the intersection at Exchange and 20th and Exchange and 18th.

3. Grades of streets shall follow as closely as practicable to the original (predevelopment) topography to minimize grading. Maximum grades and curves shall conform to the Astoria Engineering Design Standards for Roadways (Chapter 4). Where existing conditions, particularly topography, make it otherwise impracticable to provide buildable sites, steeper grades and sharper curves may be accepted by the Community Development Director or Planning Commission for land divisions, as applicable, or by the City Engineer for existing plats.

RESPONSE: No new streets are proposed in this development.

4. Where the locations of planned streets are shown on a local street network plan, the development shall implement the street(s) shown on the plan.

RESPONSE: No new streets are proposed in this development.

5. Where required local street connections are not shown on an adopted City street plan, or the adopted street plan does not designate future streets with sufficient specificity, the development shall provide for the reasonable continuation and connection of existing streets to adjacent potentially developable properties, conforming to the standards of this Code.

RESPONSE: No new streets are proposed in this development.

6. Existing street-ends that abut a proposed development site shall be extended with the development, unless prevented by environmental or topographical constraints, existing development patterns, or compliance with other standards in this Code; in such situations, the applicant must provide evidence that the environmental or topographic constraint precludes reasonable street connection.

RESPONSE: No new streets are proposed in this development.

7. Proposed streets and any street extensions required pursuant with this Section shall be located, designed and constructed to allow continuity in street alignments and to facilitate future development of vacant or redevelopable land.

RESPONSE: No new streets are proposed in this development.

8. All street names shall be approved by the City Engineer for conformance with the established pattern and to avoid duplication and confusion.

RESPONSE: No new streets are proposed in this development.

SECTION 3.015(C) - 3.070 ARE NOT APPLICABLE.

3.075. EXCEPTION TO BUILDING HEIGHT LIMITATIONS.

A. The features listed in this Section shall be exempt from the height limits established by the Code, provided the limitations indicated for each are observed.

1. Mechanical equipment and appurtenances necessary to the operation or maintenance of the building or structure itself, including chimneys, ventilators, plumbing vent stack, cooling towers, water tanks, panel or devices for the collection of solar or wind energy, and the window-washing equipment, together with visual screening for any such features.

RESPONSE: Mechanical equipment in the mechanical penthouse will be in excess of the AH-HC height limit but is exempted per this approval criteria.

2. Elevator, stair, and mechanical penthouses, fire towers, skylights, flag poles, aerials, and similar objects.

RESPONSE: The mechanical penthouse will be in excess of the AH-HC height limit but is exempted per this approval criteria.

3. Ornamental and symbolic features not exceeding 200 square feet in gross floor area including towers, spires, cupolas, belfries, and domes, where such features are not used for human occupancy.

RESPONSE: This section is not applicable to this development.

B. The total area covered by these features shall not exceed 30% of the roof area on which they are located.

RESPONSE: The total roof area covered by the mechanical penthouse on the barrel roof in which it is located is 26%.

SECTION 3.090 - 3.095 ARE NOT APPLICABLE.

3.105. LANDSCAPING.

- A. *Purpose. The purpose and intent of this section is to enhance the appearance of the City by requiring landscaping as part of commercial developments, including parking areas. These developments shall include all uses except those associated with single-family and two-family dwelling.*

RESPONSE: This section is not an approval standard.

3.110. LANDSCAPING REQUIRED.

At the time a building permit is requested for new construction, or for remodeling with a value of at least 33% of the assessed value of the structure, or in the event of a change of use or installation of new parking areas, the property shall come into compliance with the landscape requirements and a landscaping plan shall be submitted to the Community Development Director. Such landscaping plan may also be used as a site or plot plan for the development, provided all information necessary for the site or plot plan is provided. The plan shall be of sufficient scale to show existing and proposed features, proposed materials, contours (where appropriate) and other features.

RESPONSE: Please see Figure 3 for the proposed development landscape plan.

3.115. REVIEW OF LANDSCAPING PLANS.

The landscaping plan shall be reviewed by the Community Development Director to determine if it meets the quantitative requirements of the Code. Landscaping in conjunction with Uses Permitted Outright may be approved by the Community Development Director. Landscaping in conjunction with Conditional Uses shall be reviewed by the Planning Commission as part of the review under Section 11.010. In such cases, the Planning Commission may review schematic plans and the final plans may be reviewed by the Community Development Director. No Certificate of Occupancy or other final approval shall be issued by the building official or the City until the landscaping is installed as specified by the Planning Commission or Community Development Director. Minor changes in the landscape plan may be allowed by the Community Development Director, so long as they do not alter the overall character of the development.

RESPONSE: Please see Figure 3 for the proposed development landscape plan.

3.120. LANDSCAPING REQUIREMENTS.

A. *Specific requirements governing the placement and maintenance of landscape materials are as follows:*

- 1. *Landscape plant materials shall be installed to insure health and survivability. Plants chosen are known to grow successfully in the local area, with little to no maintenance requirements for long-term success.*

RESPONSE: CMH is a Planetree hospital. Planetree membership puts a premium on the natural environment and patient care. The landscape plan notes local plant materials to be part of the healing process offered at the cancer center.

- 2. *Landscape plant materials will be properly guyed and staked so as to not interfere with vehicular or pedestrian traffic.*

RESPONSE: This section is not an approval standard.

- 3. *Deciduous trees shall have a minimum caliper of one and one half (1.5) inches, and a minimum height of eight (8) feet at the time of planting,*

unless it is determined by the Community Development Director that a lesser caliper will provide the bulk and scale necessary to substantially cover the landscaped area.

RESPONSE: Please see Figure 3 for plant legend and notes. All deciduous trees are to be at least 1.5" in caliper and 8' in height at the time of planting.

- 4. Evergreen trees shall be a minimum of six (6) feet in height, fully branched, at the time of planting.*

RESPONSE: Please see Figure 3 for plant legend and notes. All evergreen trees are to be at least 8' in height per plan.

- 5. Shrubs shall be supplied in one (1) gallon containers minimum, or eight (8) inch burlap balls with a minimum spread of 12 inches.*

RESPONSE: Please see Figure 3 for plant legend. All shrubs listed are 2 gal or larger, with minimum spreads listed for sizing requirements.

- 6. Ground cover plantings shall be planted on a maximum of 18 inches on center and 18 inches between rows. Rows of plantings shall be staggered for a more effective covering. Ground cover shall be supplied in a minimum four (4) inch size container or a two and one quarter (2.25) inch size if planted on 12 inch centers.*

RESPONSE: Please see Figure 3 for plant legend and details. Ground cover plantings are 1 gal, planted at 18" triangular spacing.

- 7. Planting areas shall be designed to separate parking lots from the sidewalk and street and shall contain a mixture of trees and shrubs, except where the presence of chairwalls or public utilities makes the planting infeasible, as determined by the City Engineer, in which case concrete, stone, or other manufactured containers may be used.*

RESPONSE: Please see Figure 3 for plans. Plant areas separate the parking lots from the sidewalk and the street, and contain a variety of trees and shrubs.

- 8. Parking areas with 20 spaces or more shall have a minimum of one landscaping divider per ten (10) parking spaces. Each ten (10) parking spaces shall be bordered by a landscaped area. Such area shall consist of a curbed planter of at least three (3) feet by 16 feet, or at least 48 square feet. Each planter shall contain at least one (1) tree, along with hedge or shrub material.*

RESPONSE: Please see Figure 3 for plans. Each parking lot planter contains at least one tree, along with shrubs and ground cover.

- 9. For new construction, parking areas shall be separated from the exterior*

wall of a structure, exclusive of paved pedestrian entranceways or loading areas, by a strip of landscaping material. All planting areas shall be protected by the use of concrete bumper blocks affixed to the paving.

RESPONSE: Please see Figure 3 for plans. All planting areas are protected by concrete bumper blocks or curbs.

10. *Existing trees may be used as required landscaping. To the extent possible and practicable, required landscaping shall be within reasonable view from an improved City right-of-way.*

RESPONSE: Please see Figure 3 for plans. The required landscaping is within reasonable view from the city right-of-way on Exchange Street.

11. *All landscaping shall be maintained and kept free from trash, noxious growth, and weeds. Unkempt landscaped areas shall be considered a nuisance and shall be enforced under the applicable City code.*

RESPONSE: This section is not an approval standard.

12. *Seating areas and street furniture shall be considered part of the landscaping requirement, and shall be encouraged by the Community Development Department.*

RESPONSE: Please see Figure 3 for the seating areas in the respite garden on the south side of the building.

13. *Up to 50% of the landscaping requirement may be satisfied by the use of City right-of-ways for landscaping, as approved by the City Engineer. The property owner shall be responsible for the maintenance of such landscaping. (See City Code 2.350 through 2.353.)*

RESPONSE: Please see Figure 3 note added stating owner is responsible for maintaining right-of-way planting.

14. *Public safety should be considered in landscape designs.*

RESPONSE: This section is not an approval standard. The landscape was designed to provide pedestrian access, view corridors, screened seating and a sense of enclosure without also providing hiding places.

SECTION 3.130 - 3.240 ARE NOT APPLICABLE.

SECTION 3.300 - 3.330 are standards that are applicable and will be addressed with final construction documents that are submitted for building permit review.

**4 ARTICLE 7 - PARKING AND LOADING
APPLICABLE APPROVAL CRITERIA**

OFF-STREET PARKING AND LOADING

7.010. PARKING AND LOADING AREAS REQUIRED.

- A. Off-street parking areas and off-street loading areas meeting the applicable requirements of this Section shall be provided and maintained:
1. For each separate use in any building or structure erected after the adoption of this ordinance.
 2. For additional seating capacity, floor area, guest rooms, or dwelling units added to any existing structure or lot.
 3. When the use of the structure or portion thereof is changed.
- B. Where a structure is added to, or a portion thereof changes in use such that additional parking or loading is required, only the number of additional spaces required under Sections 7.100 and 7.160 for the area added or changed in use need be provided. Nevertheless, if the lot or structure as used prior to the addition or change of use did not have the number of parking and loading spaces required by Sections 7.100 and 7.160 and such deficiency was not lawfully nonconforming, parking for the entire building or use shall be provided as required by Sections 7.100 through 7.160.
- C. When additional parking or loading area is required or added to an existing nonconforming parking or loading area, the entire parking and loading area shall be improved as provided in Section 7.110 and landscaped setbacks from streets shall be provided as required in Section 7.170.

7.020. REDUCTION OF PARKING AREA PROHIBITED; EXCEPTION.

Off-street parking and loading areas which existed on the effective date of this ordinance or which are provided as required by this Section shall be maintained, or equivalent parking and loading areas provided; except that if this ordinance reduces the number of required off-street parking or loading spaces, an affected use may diminish its parking and loading area to the new requirements.



RESPONSES

→ This section is not an approval standard.

→ This section does not apply to this development application.



→ This section does not apply to this development application.

→ This section does not apply to this development application.



7.030. LOCATION.

- A. Off-street parking and loading areas required by this ordinance shall be provided on the same lot with the use except that:
1. In any residential zone, up to 50% of vehicle parking spaces for dwellings and other uses permitted in a residential zone may be located on contiguous lots or on a lot across a street or other right-of-way from the lot with the primary use.
 2. In non-residential zones, up to 50% of the required parking area may be located off the site of the primary use or structure provided it is within 300 feet of such site.
- B. Off-street parking is incidental to the use which it serves. As such, it shall be located in a zone appropriate to that use, or where a public parking area is a specific permitted use.

7.040. FRACTIONAL MEASUREMENTS.

When calculations for determining the number of required off-street parking or loading spaces result in a requirement of fractional space, any fraction of a space less than one-half shall be disregarded, and a fraction of one-half or greater shall be counted as one full space.

7.050. OWNERSHIP OF PARKING AND LOADING AREAS.

- A. Except as provided for joint use parking in Section 7.070, the land to be provided for off-street parking and loading areas, including driveways, aisles, and maneuvering areas shall be:
1. Owned by the owner of the property served by the parking; or
 2. In commercial and industrial zones, the parking may be provided by a permanent and irrevocable easement appurtenant to the property served by the parking; or
 3. Be leased for a minimum term of five (5) years, provided that upon expiration or termination of the lease, the parking requirements of this ordinance shall otherwise be fully met within 90 days or the use discontinued until such requirements are met.

RESPONSES



→ This section does not apply to this development application.

→ This section does not apply to this development application.

→ The proposed parking is in the same zone as the use.

 → Off-street parking calculations were performed in this manner.

→ Columbia Memorial Hospital is the property owner and applicant.

→ No easements are necessary for the parking proposed.

→ This section does not apply to this development application.



7.060. OFF-STREET VEHICLE PARKING REQUIREMENTS.

- A. Except as otherwise specifically provided in this ordinance, off-street parking spaces shall be provided in amounts not less than those set forth in Section 7.100.
- B. For any proposed use not listed in Section 7.100, the Community Development Director shall determine the parking space requirement for the most nearly similar use listed in Section 7.100 with regard to traffic generation.

7.062 SPECIAL EXCEPTIONS TO OFF-STREET VEHICLE PARKING REQUIREMENTS.A. Developed Sites Exemption.

Existing buildings which encompass all or a major portion of a lot with little or no possibility of providing off-street parking in compliance with City Code may apply to the Community Development Director for authority to participate in a program whereby, in lieu of providing required off-street parking, annual payments would be made to the City for the purpose of supporting mass transit, and development of public parking. As an alternative to making annual cash payments, the applicant may, with approval of the City Council, provide a public service of equal or greater value than the cash payment.

1. Participation in the Program.

The Director shall approve participation in the program upon a finding that the lack of required off-street parking will not result in a public safety hazard. Participation involving the provision of compensation in the form of public service in lieu of cash payments also requires the concurrence of the City Council.

2. Location.

This exception shall apply to any change of use or expansion of a use in all zones except those areas where the provision of off-street parking is otherwise exempted.

3. Compensation.a. Cash Payments.

The fee to be paid for each parking space not provided shall be \$180.00 per year.



RESPONSES



Parking is provided above the minimums set in section 7.100.



The proposed use is listed in section 7.100.



Section 7.062 does not apply to this development application.



The fee shall be paid annually on a per space basis. The number of spaces subject to a fee shall be the difference between the number of off-street spaces provided and the number required by the Astoria Development Code, or, where a Variance is issued, the number of spaces authorized by Variance.

Payments shall be made to the City of Astoria at the beginning of each year the applicant is involved in the program, and shall be made, in accordance with a payment schedule to be established by the Community Development Department.

b. Compensation in Lieu of Cash Payments.

Compensation in lieu of cash payments may be accepted only upon a finding by the City Council that there is both a need for the proposed public service, and that the value of the service is equivalent to or greater than the cash payment described in Section 3(a) above.

(Section 7.062 Added by Ordinance 93-08, 10-18-93; amended by Ordinance 96-04, 5-6-96)

B. Modification of Parking Space Requirements.

1. The applicant may propose a parking space standard that is different than the standard in Section 7.100, for review and action by the Community Development Director through a Class 1 variance, pursuant to Article 9. The applicant's proposal shall consist of a written request, and a parking analysis prepared by a qualified professional. The parking analysis, at a minimum, shall assess the average parking demand and available supply for existing and proposed uses on the subject site; opportunities for shared parking with other uses in the vicinity; existing public parking in the vicinity; transportation options existing or planned near the site, such as frequent bus service, carpools, or private shuttles; and other relevant factors. The Community Development Director may reduce the off-street parking standards for sites with one or more of the following features:
 - a. Site has a bus stop with existing or planned frequent transit service (15-minute headway or less) located adjacent to it, and the site's frontage is improved with a bus stop waiting shelter, consistent with the standards of the applicable transit service provider: Allow up to a 20 percent reduction to the standard number of automobile parking spaces;
 - b. Site has dedicated parking spaces for carpool/vanpool vehicles: Allow up to a 10 percent reduction to the standard number of automobile parking spaces;

- c. Site has dedicated parking spaces for motorcycle and/or scooter or electric carts: Allow reductions to the standard dimensions for parking spaces and the ratio of standard to compact parking spaces;
- d. Available on-street parking spaces adjacent to the subject site in amounts equal to the proposed reductions to the standard number of parking spaces.
- e. Site has more than the minimum number of required bicycle parking spaces: Allow up to a 10 percent reduction to the number of automobile parking spaces.

(Section 7.062.B added by Ordinance 14-03, 4-21-14)

C. Downtown Area.

Uses in the C-4 Zone (Central Commercial) and uses between 7th and 14th Streets in the A-2 (Aquatic Two Development) and S-2A Zones (Tourist Oriented Shoreland) are not required to provide off-street parking.

Exception: In the C-4 Zone, off-street parking and loading requirements shall apply to Lots 1, 2, 3, Block 40, McClure's Addition (south side of 600 Block Duane Street) as required by Amendment A99-02, Ordinance 99-21.

(Section 7.062.C added by Ordinance 14-03, 4-21-14)

7.070. JOINT USE OF PARKING AREAS.

- A. The Community Development Director may authorize the joint use of parking areas by the following uses or activities as a Conditional Use in every zone under the following conditions:
 - 1. Up to 50% of the off-street parking spaces required by this ordinance for a church, auditorium in a school, theater, bowling alley, night club, eating or drinking establishment may be satisfied by the off-street parking spaces provided by uses occupied only during the daytime on weekdays.
 - 2. Up to 50% of the off-street parking spaces required by this ordinance for any daytime use may be satisfied by the parking spaces provided for nighttime or Sunday uses.
 - 3. All jointly used spaces shall be located with relation to all uses relying on such spaces within the applicable distance set forth in Section 7.030.

RESPONSES



→ Section 7.070 is not an approval standard.



4. The Planning Commission must find that there is no substantial conflict in the principal operating hours of the buildings or uses for which joint use of off-street parking facilities is proposed.
5. A properly drawn legal instrument executed by the parties concerned with joint use of off-street parking facilities, approved as to form and manner of execution by Legal Counsel, shall be filed with the Community Development Director. Joint use parking privileges shall continue in effect only so long as such an instrument, binding on all parties, remains in force. If such instrument becomes legally ineffective, then parking shall be provided as otherwise required in this ordinance within 60 days.

7.080. CLASSIFICATION OF USES FOR PURPOSES OF JOINT USE PARKING.

A. The following uses are considered daytime uses for purposes of Section 7.070:

1. Bank or other financial institution.
2. Business service establishment.
3. Clothing, shoe repair, or service establishment.
4. Household equipment or furniture store.
5. Manufacturing or wholesale building.
6. Personal service establishment.
7. Retail store.
8. Other similar primarily daytime uses as determined by the Community Development Director.

B. The following uses are considered nighttime or Sunday uses for purposes of Section 7.070:

1. Auditoriums incidental to a public or private school.
2. Church.
3. Eating and drinking establishment, only open after 5:00 p.m.
4. Night Club.
5. Theater.

RESPONSES

→ Section 7.080 is not an approval standard.

6. Other similar primarily nighttime uses as determined by the Community Development Director.

7.090. OFF-STREET LOADING.

- A. Except as otherwise specifically provided in this ordinance, off-street loading shall be provided in amounts not less than those set forth in Section 7.160.
- B. A parking area meeting the requirements of Sections 7.100 through 7.110 may also be used for loading when the use does not require a delivery vehicle which exceeds a combined vehicle and load rating of 20,000 pounds, and when the parking area is within 25 feet of the building or use which it serves.
- C. Downtown Area

Uses in the C-4 Zone (Central Commercial) and uses between 7th and 14th Streets in the A-2 (Aquatic Two Development) and S-2A Zones (Tourist Oriented Shoreland) are not required to provide off-street loading.

Exception: In the C-4 Zone, off-street parking and loading requirements shall apply to Lots 1, 2, 3, Block 40, McClure's Addition (south side of 600 Block Duane Street) as required by Amendment A99-02, Ordinance 99-21.

(Section 7.090.C added by Ordinance 14-03, 4-21-14)



RESPONSES



The proposed development meets section 7.160.



The loading area can accommodate large vehicles. The loading area for deliveries is the mobile PET/CT parking pad with deliveries coming through the west entry.



7.090(C) does not apply to this application.



Use Categories	Minimum Parking per Land Use (Fractions are rounded up to the next whole number.)
COMMERCIAL CATEGORIES	
Animal hospital or kennel	1 space per 300 sq. ft. gross floor area
Automotive repair & service, gas station	1 space per 1,000 sq. ft. gross floor area
Bed and Breakfast, Home Stay Lodging, Inn	1 additional space for each bedroom used for transient lodging Plus spaces required for associated uses such as assembly areas or restaurant.
Daycare, Family/Home	1 space, plus required parking for dwelling
Daycare Center	1 space per employee
Eating and Drinking / Restaurant	1 space per 500 sq. ft. if no seating; 1 space per 250 sq. ft. with seating.
Educational Services, not a school (e.g., tutoring or similar services, excluding single student tutoring facilities)	1 space per 300 sq. ft. gross floor area
Home Occupation with customers and/or non-resident employees	1 additional space per anticipated customer/employee at a specific time in excess of one person at a time
Hotels, Motels, and similar uses	1 space per guest room. See also, parking requirements for associated uses, such as restaurants, entertainment uses, drinking establishments, assembly facilities.
Laundromat and dry cleaner	1 space per 350 sq. ft. gross floor area
Mortuary/Funeral Home	1 space per 300 sq. ft. gross floor area
Offices: General, medical/dental, professional	1 space per 500 sq. ft. gross floor area
Personal Services (i.e. salon, spa, barber, animal grooming, out-patient veterinary services)	1 space per chair, table, or booth for customers
Repair or Service other than automotive	1 space per 500 sq. ft. gross floor area
Retail Sales, General Merchandise	1 space per 500 sq. ft. gross floor area



RESPONSES



 The proposed development is a medical office use. The required minimum parking for the 19,657 square foot building is 39 spaces. 58 spaces are provided. Please note that many other jurisdictions have code minimum parking for medical use of 2.0 to 2.7 spaces per 1,000 square feet and maximums of 3.33 to 4.0 spaces per 1,000 square feet. The proposed parking ratio falls comfortably in that range at 2.95 spaces per 1,000 square feet.

Use Categories	Minimum Parking per Land Use (Fractions are rounded up to the next whole number.)
OTHER CATEGORIES	
Accessory Uses	Parking standards for accessory uses are the same as for primary uses, but are pro-rated based on the percentage of estimated overall parking demand, subject to City review and approval.
Temporary Uses	Parking standards for temporary uses are the same as for primary uses, except that the Community Development Director or Planning Commission, as applicable, may reduce or waive certain development and designs standards for temporary uses.
Transportation and Communications Facilities (operation, maintenance, preservation, and construction)	None, except where temporary parking is required for construction staging areas

(Section 7.100 amended by Ordinance 14-03, 4-21-14)

7.105. BICYCLE PARKING.

A. Standards.

Bicycle parking spaces shall be provided for new development, change of use, and major renovation, at a minimum, based on the standards in Table 7.105. Major renovation is defined as construction valued at 25% or more of the assessed value of the existing structure.

Where an application is subject to Conditional Use Permit approval or the applicant has requested a reduction to an automotive parking standard, pursuant to Section 7.062, the Community Development Director or Planning Commission, as applicable, may require bicycle parking spaces in addition to those in Table 7.105.

Table 7.105: Minimum Required Bicycle Parking Spaces

RESPONSES



→ The proposed development meets the requirements of 7.105.



Use	Minimum Number of Spaces	Long and Short Term Bicycle Parking Percentages
Multi-family Residential Dwelling with 4 or more dwelling units	1 bike space per 4 dwelling units	75% long term 25% short term
Commercial	1 bike spaces per primary use or 1 per 10 vehicle spaces, whichever is greater	50% long term 50% short term
Industrial	1 bike spaces per primary use or 1 per 20 vehicle spaces, whichever is greater	25% long term 75% short term
Parks (active recreation areas greater than 10,000 sq. ft.)	4 bike spaces per 10,000 sq. ft.	100% short term
Schools (all types)	1 bike spaces per 4 classrooms	50% long term 50% short term
Institutional Uses and Places of Worship	1 bike space per 20 vehicle spaces	100% short term
Other Uses	2 bike spaces per primary use or 1 per 10 vehicle spaces, whichever is greater	50% long term 50% short term

B. Design and Location.

1. All bicycle parking shall be securely anchored to the ground or to a structure.
2. All bicycle parking shall be designed so that bicycles may be secured to them without undue inconvenience, including being accessible without removing another bicycle.
3. All bicycle parking should be integrated with other elements in the planter strip when in the public right-of-way.
4. Direct access from the bicycle parking area to the public right-of-way shall be provided at-grade or by ramp access, and pedestrian access shall be provided from the bicycle parking area to the building entrance.
5. Bicycle parking shall not impede or create a hazard to pedestrians or vehicles, and shall not conflict with the vision clearance standards of City Code Section 6.100.



RESPONSES



There are 58 parking spaces provided and therefore 6 bike spaces are provided. 3 long-term and 3 short term. The three short term spaces will be located at the main entry to the cancer center and the 3 long term spaces will be located near the west staff entry.



The bike parking systems will be bolted to the concrete.



Standard bike parking equipment will be specified.



No bike parking is proposed in the public right-of-way.



Direct access from both the long-term and short-term bike parking is provided via 6 foot sidewalk and ADA accessible ramp to the public right-of-way. This is true for access to the main building entrance or the staff west entrance as well.



Proposed development complies.

6. Short-term bicycle parking.
- a. Short-term bicycle parking shall consist of a stationary rack or other approved structure to which the bicycle can be locked securely.
 - b. If more than 10 short-term bicycle parking spaces are required, at least 50% of the spaces must be sheltered. Sheltered short-term parking consists of a minimum 7-foot overhead clearance and sufficient area to completely cover all bicycle parking and bicycles that are parked correctly.
 - c. Short-term bicycle parking shall be located within 50 feet of the main building entrance or one of several main entrances, and no further from an entrance than the closest automotive parking space.

6. Long-term bicycle parking.

Long-term bicycle parking shall consist of a lockable enclosure, a secure room in a building on-site, monitored parking, or another form of sheltered and secure parking.

C. Exemptions.

This Section does not apply to single-family, two-family, and three-unit multi-family housing, home occupations, and agricultural uses. The Community Development Director or Planning Commission as applicable may exempt other uses upon finding that, due to the proximity of public bicycle parking facilities, the nature of the use, or its location, it is unlikely to have any patrons or employees arriving by bicycle.

(Section 7.105 added by Ordinance 14-03, 4-21-14)

7.110. PARKING AND LOADING AREA DEVELOPMENT REQUIREMENTS.

All parking and loading areas required under this ordinance, except those for a detached single-family dwelling on an individual lot unless otherwise noted, shall be developed and maintained as follows:

(Section 7.110 amended by Ordinance 14-03, 4-21-14)

A. Location on site.

Required yards adjacent to a street, shall not be used for parking and loading areas unless otherwise specifically permitted in this ordinance. Side and rear yards which are not adjacent to a street may be used for such areas when developed and maintained as required in this ordinance.

RESPONSES



→ Stationary rack is proposed.

→ Less than 10 are required.

→ Short-term parking is located at the main building entrance.

→ Long-term bike parking will be a lockable, weatherproof enclosure near the west staff entrance to the building.



→ No exemptions necessary.

→ Required yards and setbacks are not used for loading in this application.



B. Surfacing.

All parking and loading areas and driveways thereto shall be paved with asphalt, concrete or other hard surface approved by the City Engineer. Parking and loading areas shall be adequately designed, graded, and drained.

C. Bumper guards or wheel barriers.

Permanently affixed bumper guards or wheel barriers are required and shall be so installed that no portion of a vehicle will project into a public right-of-way or over adjoining property. The area beyond the wheel barriers or bumper guards shall be surfaced as required in Section 7.110(B) or landscaped.

D. Size of parking spaces and maneuvering areas.

The parking area, each parking space, and all maneuvering areas shall be of sufficient size and all curves and corners of sufficient radius as determined by the City Engineer to permit the safe operation of a standard size vehicle subject to the following minimum requirements:

1. Full size parking spaces shall be nine and one half (9.5) feet wide and 20 feet long.
2. Compact parking spaces shall be eight and one half (8.5) feet wide and 16 feet long for no more than 50% of the parking spaces required.

An increase to 75% compact may be approved administratively by the Community Development Director upon a finding that anticipated use would not require compliance. An increase greater than 75% may be approved by the Community Development Director as a Class 1 Variance in accordance with Article 9.

3. Where a landscaped area, fence, or wall is adjacent to a parking space, the parking space shall be ten (10) feet wide.
4. A maximum of 2.5' of a parking stall required length may extend beyond the wheel barrier into a landscaped area. The parking stall shall not extend into a pedestrian walkway area.

(Section 7.110.D amended by Ordinance 14-03, 4-21-14)

E. Access.

RESPONSES



→ The proposed development meets this required and will be reviewed as such during the building permitting process.

→ No condition exists with parking where a vehicle would project over a public-right-of-way or adjoining property.



→ This standard is met in the proposed development.

→ This standard is met in the proposed development.

→ This standard is met in the proposed development.

→ This standard is met in the proposed development.



Parking or loading areas having more than four (4) spaces shall be designed so that vehicles do not back into public streets, or do not use public streets for maneuvering.

All entrances and exits onto public streets shall first have a Driveway Permit from the Engineering Department and shall be designed and constructed to City standards.

F. Lighting.

Parking or loading areas that will be used at nighttime shall be lighted. Outdoor lighting shall be directed away from any adjacent residential zone or public street.

G. Landscaping.

1. Landscaping shall be provided as required in Section 7.170 and Section 3.105 through 3.120.
2. Required landscaped yards shall not be used for parking.

(Section 7.110.G amended by Ordinance 14-03, 4-21-14)

H. Additional Requirements.

1. Directional signs and pavement marking shall be used to control vehicle movement in parking area.

(Section 7.110.H amended by Ordinance 14-03, 4-21-14)

I. Aisle Widths

Aisles with parking adjacent on one or both sides, depending on angle of parking spaces:

	<u>Minimum Width</u>
0 - 40 degrees	12 feet
41 - 45 degrees	13 feet
46 - 55 degrees	15 feet
56 - 70 degrees	18 feet
71 - 90 degrees	24 feet



RESPONSES



No parking or loading backs or maneuvers in the public street.



Outdoor lighting is provided and is directed away from Exchange Street.



Please see Chapter 3 for responses.



No yards are used for parking.



Pavement signage will be provided.



Aisle width standards are met in the proposed development.

7.120. DRIVEWAY DEVELOPMENT STANDARDS.

All driveways providing access to parking spaces and loading areas required under this ordinance, including those for a single-family dwelling on a lot, shall conform to the Astoria City Code Sections 2.050 through 2.100 and Development Code Section 3.008.D, in addition to requirements in the Astoria Engineering Design Standards for Roadways (Chapter 4).

(Section 7.120 amended by Ordinance 14-03, 4-21-14)

7.130. OUTDOOR STORAGE AREA SURFACING.

- A. Where commercial, industrial, or shoreland zones permit outdoor storage, or if such storage is permitted as part of a Conditional Use in any zone, such storage areas and any access driveway shall be paved and shall have plans for off-site drainage approved by the City.

7.140. PARKING PLAN REQUIRED.

Plans, at a workable scale, for all parking and loading areas required under this Section, shall be submitted to the Community Development Director for approval prior to issuance of a permit; or, if no building permit is required, at the time of application for a driveway permit; or, if no such permit is required, prior to commencing any paving or use of the parking or loading area. No such work or use shall commence prior to approval by the City of the plans required by this Section.

7.150. ACCESSIBLE PARKING SPACES.

- A. Effective September 1, 1990, existing and new parking spaces for disabled persons shall be required by law at all public and government buildings.
- B. The size, location, dimension, and marking for accessible parking spaces shall be in accordance with current State and Federal regulations for accessible parking facilities.

(Section 7.150 amended by Ordinance 14-03, 4-21-14)

RESPONSES



→ The driveway element of this application meets this and the references standards.

→ No outdoor storage is proposed.

→ A full scale parking plan will be provided at the time of building permit review.



→ Accessible parking spaces are provided.



7.160. MINIMUM LOADING SPACE REQUIREMENTS.

USE OR GROSS SQUARE FOOTAGE OF FLOOR AREA	MINIMUM NO. OF SPACES	MINIMUM SIZE OF SPACE		
		WIDTH	LENGTH	HEIGHT

A. Multi-Family Dwelling Units.

0 - 49 Units	0	12 ft	19 ft	12 ft
50 - 99 Units	1	"	"	"
100 - 199 Units	2	"	"	"
200 and over Units	3	"	"	"

If a recreational or service building is provided, at least one of the required loading spaces shall be located in conjunction with the recreational or service building.

B. For Buildings Used Entirely for Office Occupancy.

Under 5,000 sq ft	0	12 ft	30 ft	14 ft
5,000 - 59,999 sq ft	1	"	"	"
60,000 - 249,999 sq ft	2	"	"	"

For each additional 100,000 square feet of any portion thereof over 250,000 square feet, one additional loading space.

C. Commercial, Non-office, Public and Semi-Public.

Under 5,000 sq ft	0	12 ft	55 ft	14 ft
5,000 - 59,999 sq ft	1	"	"	"
60,000 - 249,999 sq ft	2	"	"	"

For each additional 100,000 square feet of any portion thereof over 250,000 square feet, one additional loading space.

D. Industrial.

Under 5,000 sq ft	0	12 ft	55 ft	14 ft
5,000 - 99,999 sq ft	1	"	"	"
100,000 - 239,999 sq ft	3	"	"	"
240,000 - 319,000 sq ft	5	"	"	"
320,000 - 399,000 sq ft	6	"	"	"
400,000 - 489,999 sq ft	7	"	"	"
490,000 - 579,999 sq ft	8	"	"	"
580,000 - 669,999 sq ft	9	"	"	"
670,000 - 759,999 sq ft	10	"	"	"

For each additional 100,000 square feet or any portion thereof over 760,000 square feet, an additional loading space is required.

RESPONSES



→ One loading space is provided and is the mobile PET/CT parking pad.



7.170. LANDSCAPING OF OUTDOOR STORAGE OR PARKING AREAS.

A minimum of 5% of the gross parking lot area shall be designed and maintained as landscaped area, subject to the standards in Sections 3.105 through 3.120. This requirement shall apply to all parking lots with an area of 600 square feet or greater. Approved sight obscuring fences or vegetative buffers shall be constructed where commercial parking lots abut Residential Zones. The minimum 5% landscaping shall be counted as part of the total landscaping required for the property.

(Section 7.180, Parking in the Downtown Area, Exception added by Ordinance 99-21, 11-1-99; Section 7.180 deleted by Ordinance 14-03, 4-21-14)

RESPONSES



→ More than 5% of the gross parking lot area is landscaped per Article 3. See landscape plan.



5 ARTICLE 8 - SIGNS
APPLICABLE APPROVAL CRITERIA

SIGN REGULATIONS

8.010. PURPOSE.

The purpose of this Section is to regulate the number, size, placement and physical characteristics of signs in order to achieve the following objectives:

1. The maintenance of public safety and traffic safety by ensuring that signs are appropriately designed, constructed, installed and maintained.
2. The enhancement of the operation of businesses in the City by promoting the reasonable, orderly and effective display of signs.
3. The enhancement of the City's physical appearance by promoting signs which are visually compatible with their surroundings and preserve the visual integrity of the area.

(Section 8.010.3 amended by Ordinance 04-04, 5-3-04)

8.020. ADOPTION OF UNIFORM SIGN CODE.

The City of Astoria enforces the State building code per ORS Chapter 455 and the rules adopted there under by reference, except as modified in this Code.

(Section 8.020 amended by Ordinance 04-04, 5-3-04)

8.030. CONFORMANCE.

No sign may be erected or allowed to remain unless it conforms with the regulations of Sections 8.010 through 8.180. Sign permits, as required by 8.060, must be approved prior to the placement of a sign. All signs in historic districts, or in conjunction with historic buildings or sites subject to the Historic Landmarks Code must be approved through the review process outlined in Sections 6.050 and 6.090.

(Section 8.030 amended by Ordinance 04-04, 5-3-04)



RESPONSES

→ This section is not an approval standard.



→ This section is not an approval standard.

→ All signs will conform to Article 8.



8.040. EXEMPT SIGNS.

A. The following signs are permitted and are exempt from the requirements of this Code:

1. Building plaque, cornerstone, or similar building identification which is an integral and normal part of a building.
2. House and building numbers, not to exceed four (4) square feet, with numbers not exceeding 12 inches in height.
3. Decorative banners in residential zones not exceeding six (6) square feet.
4. Official informational signs, traffic signs, kiosks, signals, notices, and decorative and event banners.
5. Historical markers erected or maintained by public authority or by a recognized historical organization.
6. Historical signs, and reproductions of historic signs.

(Section 8.040.A.6 amended by Ordinance 04-04, 5-3-04)

7. A wall sign for an approved home occupation not exceeding one (1) square foot.
8. Directional signs, each not exceeding four (4) square feet.
9. Flags of local, state, or national origin.
10. Signs located within a building, except window signs.
11. Informational signs, such as hours of operation, accepted cards, and similar signs not exceeding one (1) square foot for groups of related signage. Open and closed signs not exceeding 1.5 square feet.

(Section 8.040.A.11 amended by Ordinance 04-04, 5-3-04)

12. Signs, not exceeding 24 square feet, in residential zones which are used for the identification of public and semi-public uses.
13. One short term real estate sign for each street frontage located on the premises for sale, lease or rent, not exceeding six (6) square feet, provided they are removed within 14 days after the transaction has been completed.

RESPONSES



→ The proposed development will have exempt signage per #11.



(Section 8.040.A.13 amended by Ordinance 04-04, 5-3-04)

14. Political signs, located on private property. Political signs related to an election shall be removed 14 days after the election. Political signs not meeting this exemption shall comply with the sign code regulations and permit process.

(Section 8.040.A.14 amended by Ordinance 04-04, 5-3-04)

15. Signs located on buildings in aquatic zones not visible from a public street or right-of-way, not exceeding 32 square feet.
16. Signs affixed to the face of individual gas pumps located at gasoline service station.
17. Portable signs which are located within a street right-of-way in accordance with City Code Section 5.060.

(Section 8.040.A.17 added by Ordinance 00-11, 12-4-00)

18. Short term signs, in the nature of decorations, clearly incidental to and customarily and commonly associated with any national, local, or religious holiday.

(Section 8.040.A.18 added by Ordinance 04-04, 5-3-04)

8.050. PROHIBITED SIGNS.

A. The following signs are prohibited:

1. Strobe lights and signs containing strobe lights.
2. Spot lights and beacons, except for special community wide events by permit.
3. Signs which flash, revolve, rotate, swing, undulate or otherwise attract attention through the movement or flashing of parts of the sign, including inflatable signs, large balloons, flags, pennants, or similar devices.

This prohibition does not include the following signs:

- a. barber poles of maximum of 4' in total fixture height may rotate;
- b. changeable text signs;
- c. time and temperature signs.

RESPONSES



→ No prohibited signs are proposed.



(Section 8.050.A.3 amended by Ordinance 04-04, 5-3-04; amended by Ordinance 12-03, 1-3-12)

4. *(Section 8.050.A.4 deleted by Ordinance 04-04, 5-3-04)*
5. Abandoned or deteriorated signs.
6. Public address systems or sound devices for advertising purposes.
7. Backlit awning signage is prohibited in the area bounded by Exchange Street on the south, the pierhead line on the north, 8th Street on the west, and 16th Street on the east; or within the Gateway Master Plan Area.

(Section 8.050.A.7 amended by Ordinance 04-04, 5-3-04)

8. No unofficial sign which purports to be, is an imitation of, or resembles an official traffic sign or signal, or which attempts to direct the movement of traffic, or which hides from view any official traffic sign or signal.
9. Animation Signs.

(Section 8.050.A.9 added by Ordinance 04-04, 5-3-04)

10. Changeable text signs on a vacant lot.

(Section 8.050.A.10 added by Ordinance 04-04, 5-3-04)

11. Off-premise changeable text signs.

(Section 8.050.A.11 added by Ordinance 04-04, 5-3-04)

8.060. SIGN PERMITS.

A. Sign Permit Required.

A sign permit is required for the erection of any new sign or the structural alteration of an existing sign, except those signs that are exempt in Section 8.040. A sign permit is required for modification or alteration of the sign face, or any portion of the sign or supporting structure.

(Section 8.060.A amended by Ordinance 04-04, 5-3-04)

RESPONSES



→ A sign permit will be requested for the proposed signs.



B. Required Information For a Sign Permit.

For purposes of review by the Community Development Director, a scale drawing of the proposed sign shall be submitted. The drawing shall include:

1. The dimensions of the sign;
2. Location of the sign;
3. Any structural elements of the proposed sign; and
4. The size, location, and dimensions of any other sign(s) located on the applicant's building or property.

(Section 8.060.B.4 amended by Ordinance 04-04, 5-3-04)

5. A site plan indicating the dimension of the building frontage and/or site frontage.

(Section 8.060.B.5 added by Ordinance 04-04, 5-3-04)

6. In addition to any other application requirements listed above, all changeable text sign applications shall include the following:
 - a. Manufacturer's information on the operation, illumination, and ability of the sign to comply with the regulations and standards in this Code.
 - b. Diagram with at least two sample messages for the proposed activity utilizing the lighting capabilities of the proposed sign.

(Section 8.060.B.6 added by Ordinance 04-04, 5-3-04)

C. Sign Permit Fee.

The fee for a sign permit shall be established by Resolution.

(Section 8.060.C amended by Ordinance 04-04, 5-3-04)



RESPONSES



This design review application has general information for the one monument sign and one building sign proposed for the project. Detailed dimensions, materials, and text will be available with the permit documents at the time of building permit application. All sign requirements will be followed per Article 8. Part of the reason for this is that CMH and OHSU still need to agree and approve the naming for this building.



This section is not an approval standard.

8.070. GENERAL SIGN REGULATIONS.

The following general provisions shall govern all signs, in addition to all other applicable provisions pertaining to signs:

A. Sign Face Area.

1. The area of sign faces enclosed in frames or cabinets is determined based on the outer dimensions of the frame or cabinet surrounding the sign face [See 8.120(A.1), Figure 1]. Sign area does not include foundations, supports, and other essential structures which do not serve as a backdrop or border to the sign. Only one (1) side of a double faced sign is counted in measuring the sign face area, except for a double faced changeable text sign.

(Section 8.070.A.1 amended by Ordinance 04-04, 5-3-04)

2. When signs are constructed of individual elements attached to a building wall, the sign area is determined by calculating the area of an imaginary rectangle drawn around the sign elements [See 8.120(A.2), Figure 2].
3. Several businesses may use one sign as long as the area they each use does not exceed their own allowable square footage, and the total area of the sign does not exceed that allowed in the zone.
4. The area of sign faces for round or three dimensional signs is determined by the maximum sign face area visible at one time.
5. When a sign is incorporated into an awning or marquee, only the sign area as determined by a perimeter drawn around the individual elements is counted as the sign face.
6. For sign structures containing multiple sign modules oriented in the same direction, the sign area is determined by calculating the area of an imaginary rectangle drawn around the sign elements [See 8.120(A.3), Figure 3].
7. Sign area square footage is based on frontage. Freestanding and monument signs are based on the site frontage, all other signs are based on the building frontage.

(Section 8.070.A.7 added by Ordinance 12-03, 1-3-12)

B. Height of Signs.



RESPONSES

→ This section is not an approval standard.

→ This section is not an approval standard.

→ This section is not an applicable approval standard.

→ This section is not an approval standard.

→ This section is not an applicable approval standard.

→ This section is not an applicable approval standard.

→ This section is not an approval standard.



The overall height of a sign or sign structure is measured from the existing grade directly below the sign to the highest point of the sign or sign structure [See 8.120(B.1), Figure 4].

C. Clearances.

Clearances are measured from the existing grade directly below the sign to the bottom of the sign structure enclosing the sign face [See 8.120(C.1), Figure 5].

D. Corner Signs.

Corner signs facing more than one (1) street shall be assigned to a frontage by the applicant. The sign must meet all provisions for the frontage it is assigned to.

E. Sign Placement.

1. Placement. All signs and sign structures shall be erected and attached totally within the site except when allowed to extend into the right-of-way.

2. Frontages. Signs allowed based on the length of one (1) site frontage may not be placed on another site frontage.

a. Exception.

If a portion of a building facade or site line is more suited for signage than the allowable frontage, an applicant may choose to use that building facade or site line in lieu of the allowable frontage. The square footage of the sign shall be calculated on the length of the newly selected building facade or site line or on the allowable frontage, whichever is smaller. In choosing this exception, the applicant shall relinquish the right to install signage on the other allowable frontage unless a variance is granted.

(Section 8.070.E.2 amended by Ordinance 04-04, 5-3-04)

3. Vision Clearance Areas. No sign may be located within a vision clearance area as defined in Section 3.045. No support structure(s) for a sign may be located in a vision clearance area unless the combined total width is 12 inches or less and the combined total depth is 12 inches or less.

4. Vehicle Area Clearances. When a sign extends over a private area where vehicles travel or are parked, the bottom of the sign structure shall be at least 14 feet above the ground. Vehicle areas include driveways, alleys, parking lots, and loading and maneuvering areas.

RESPONSES



→ This section is not an approval standard.

→ This section is not an approval standard.

→ This section is not an applicable approval standard.

→ The monument sign proposed is totally within the site property.

→ The north frontage length will be the basis.



→ Vision clearance areas are met with the placement of the monument sign per 3.045.

→ This section is not an applicable approval standard.



5. Pedestrian Area Clearances. When a sign extends over sidewalks, walkways or other spaces accessible to pedestrians, the bottom of the sign structure shall be at least eight (8) feet above the grade except for pedestrian signs located below marquees, canopies, or awnings which shall be at least seven and one half (7.5) feet above the grade.

(Section 8.070.E.5 amended by Ordinance 04-04, 5-3-04)

6. Required Yards and Setbacks. Signs may be erected in required yards and setbacks.

F. Signs Not to Constitute a Traffic Hazard.

Signs or sign supporting structures shall not be located so as to detract from a motorist's view of vehicular or pedestrian traffic or a traffic sign.

G. Glare.

All signs shall be so designed and located so as to prevent the casting of glare or direct light from artificial illumination upon adjacent publicly dedicated roadways and surrounding property.

H. Removal of Abandoned Sign.

It is the responsibility of the property owner to remove any abandoned sign within 90 days of cessation of use.

I. Materials.

A sign subject to a permit shall meet the material and construction methods requirements of the Uniform Sign Code.

J. Maintenance.

All signs, together with their supporting structures, shall be kept in good repair and maintenance. Signs shall be kept free from excessive rust, corrosion, peeling paint, or other surface deterioration. The display surfaces and vegetation surrounding all signs shall be kept in a neat appearance.

K. Through the Block Signage.

Buildings which contain frontage on two parallel arterial streets, or on an arterial street and a waterway, shall be entitled to twice the allowable total square footage for the zone in which it is placed. [See 8.070(E.2)]. This double allowance affects only the overall total square footage for the site. The maximum square footage of each

RESPONSES



→ This section is not an applicable approval standard.

→ The monument sign will be erected partially in the front yard setback requirement as allowed per this rule.

→ The monument sign will not constitute a traffic hazard.

→ No glare will be case from the monument sign.



→ No abandon signs will remain.

→ The monument will be construction of approved materials.

→ CMH will maintain the sign.

→ Not an applicable approval standard.



individual sign, the square footage for the frontage, the number of signs, location, and other attributes of the sign are not affected by this allowance.

(Section 8.070.K amended by Ordinance 04-04, 5-3-04)

8.080. SPECIFIC SIGN REGULATIONS (Applicable to All Zones).A. Wall or Roof Signs.

1. Projection. Signs may project a maximum of 12 inches from the face of the building to which they are attached, provided the lowest portion of the sign is at least eight (8) feet above grade. Any portion lower than eight (8) feet may only project four (4) inches.
2. Extension above roof line. Unless otherwise specified, signs may not project more than four (4) feet above the eaves of the primary roof structure of a pitched roofed building, or more than two (2) feet above the eave or parapet of the primary roof structure of a flat roofed building.

B. Marquee, Canopy or Awning Signs.

1. Projection. Signs attached to the face of a marquee, canopy, or awning shall not project more than three (3) inches from the surface.
2. Projection of pedestrian signs. Pedestrian signs may not project beyond or above the face of the marquee, canopy, or awning.
3. Height. Pedestrian signs shall have a maximum face height of nine (9) inches.
4. Clearance above grade. The lowest portion of a sign attached to a marquee, canopy, or awning shall not be less than seven and one-half (7.5) feet above grade.

C. Projecting Signs.

1. Projection. Signs may project from the face of the building to which they are attached as follows:
 - a. A maximum of one (1) foot if located eight (8) feet above grade;
 - b. Two (2) feet if located nine (9) feet above grade; or
 - c. Four and one-half (4.5) feet if located ten (10) feet or more above grade.
 - d. Six (6) feet if located 12 feet or more above grade.

They must be mounted at a right angle to the plane of the building facade except signs located on the corner of a building.



RESPONSES

→ The one wall sign proposed is on the south elevation, is above 8 feet above grade and will not project more than 12 inches.

→ The wall sign will not project above the roof line.

→ No marquee, canopy, or awning signs are proposed.



→ The proposed sign will not exceed these projection requirements.



(Section 8.080.C.1 amended by Ordinance 04-04, 5-3-04)

2. Height and extension above roof line. Signs shall not extend above the eaves of the primary roof structure of any pitched roof building to which they are attached, or be lower than eight (8) feet above grade. Signs may extend a maximum of two (2) feet above the facade or parapet of the primary roof structure of any flat roofed building.
3. Clearance above grade. Projecting signs not occurring under marquees, canopies, or awnings shall maintain a clearance of not less than eight (8) feet from the underlying sidewalk.
4. Angle of sign. The angle between the two sides of a projecting sign may not be greater than 30°, and the two sides may not be visible at the same time from adjacent properties or streets. Signs that are greater than 30° shall be counted as two signs in number and square footage.

(Section 8.080.C.4 added by Ordinance 04-04, 5-3-04)

D. Freestanding Signs.

1. Signs may extend into the right-of-way a maximum of four and one-half (4.5) feet, and shall not be less than ten (10) feet from the underlying sidewalk.

(Section 8.080.D.1 amended by Ordinance 04-04, 5-3-04)

2. Diagonal corner signs may extend into the right-of-way to a point that is determined by extending a line from the maximum points allowed for a standard projecting sign on the same site frontage.
3. Supporting structures shall be located within the property line.

E. Wall Graphics.

1. Except as noted in Section E.3 below, designs shall be evaluated by the Planning Commission on a case by case basis in order to determine appropriateness to the area. The Planning Commission may approve, deny, or modify requests, in accordance with Sections 9.010 through 9.100, based on their evaluation of:
 - a. The appropriateness of the work in terms of color, scale, location and design; and
 - b. The impact on surrounding buildings, views and vistas.

(Section 8.080.E.1 amended by Ordinance 04-04, 5-3-04)

RESPONSES



→ The proposed wall sign will not project above the roof line.

→ The proposed sign will be above 8 feet above grade.

→ The proposed sign will not be angled.

→ No freestanding signs are proposed.



→ No wall graphics are proposed.



2. The square footage of a wall graphics is not calculated as part of the allowable area of signage or number of signs associated with a business site, use, or activity.
3. A wall graphic proposed to be located on a historic structure or site, adjacent to or across the right-of-way from a historic structure or site, within a National Register Historic District, or adjacent to or across the right-of-way from a National Register Historic District shall be evaluated by the Historic Landmarks Commission on a case by case basis in order to determine appropriateness to the area. The Historic Landmarks Commission may approve, deny, or modify requests, in accordance with Sections 9.010 through 9.100, based on their evaluation of:
 - a. The appropriateness of the work in terms of color, scale, location and design; and
 - b. The impact on surrounding historic structures or sites; and
 - c. The impact on surrounding buildings, views and vistas.

(Section 8.080.E.3 added by Ordinance 04-04, 5-3-04)

F. Removal of Signs.

When a sign is proposed, or when roadways are widened, or other improvements made in the right-of-way, which create unsafe conditions due to a sign extending into the right-of-way, the City Engineer may protect the public safety by requiring the sign to be modified or removed. The modification or removal shall be at the owner's expense.

G. Permanent Window Signs.

Permanent window signs shall be included and calculated as "wall signs" except as follows:

1. Number. Permanent window signage less than twelve (12) square feet is exempt from the total number of signs per frontage.
2. Area. All permanent window signage shall be included in the total allowable wall sign area for the business, building, use or activity.

(Section 8.080.G added by Ordinance 04-04, 5-3-04)



RESPONSES



→ CMH will remove any applicable sign.

→ No permanent window signs are proposed.



H. Directional Signs.

Directional signs, where allowed, shall meet the provisions listed below. Directional signs shall be designed only for non-changing messages or displays.

1. Maximum Sign Face Area. Six (6) square feet.
2. Types of Signs Allowed. Freestanding, wall, projecting, monument.
(Section 8.080.H.2 amended by Ordinance 04-04, 5-3-04)
3. Maximum Height. Freestanding **or monument** not to exceed 42 inches; wall not to exceed eight (8) feet; and projecting not to exceed ten (10) feet.
(Section 8.080.H.3 amended by Ordinance 04-04, 5-3-04)
4. Extensions into Right-of-way. Not Allowed.
5. Lighting. Indirect or Internal.
6. Flashing Lights. Not allowed.
7. Changeable Text Signs or Electronic Message Centers. Not Allowed.
(Section 8.080.H.7 amended by Ordinance 04-04, 5-3-04)
8. Moving or Rotating Parts. Not Allowed.

I. Clear Vision Area.

Refer to Section 3.045.

J. Signs on Historic Properties or in Historic Districts.

Refer to Article 6. Also see specific requirements as noted in various sections of Article 8.

(Section 8.080.J amended by Ordinance 04-04, 5-3-04)

K. Temporary Signs.

1. Time Limit. Temporary signs and support structures, if any, must be removed within 30 days of date of erection. Temporary signs erected for longer than 30 days are considered permanent signs and subject to the regulations for permanent signs.

RESPONSES



→ No directional signs are proposed.



→ Clear vision areas as described in 3.045 are met with the proposed design.

→ This section is not an applicable approval standard.

→ No temporary signs are proposed.



2. Waiting Period. There shall be a 30 day waiting period before another exterior temporary sign, including banners, shall be displayed.
3. Attachment. Temporary signs may not be permanently attached to the ground, buildings, or other structures. Temporary signs are not required to be located on a frontage.
3. Lawn Signs. Lawn signs may not be greater than nine (9) square feet in area and may not be over 40 inches in height.
4. Banners. One banner is allowed per building frontage and may not exceed 24 square feet.
5. Opening or Coming Soon Signs.
 - a. Temporary signs indicating an upcoming opening may not exceed 24 square feet and shall comply with the other requirements for temporary signage.
 - b. Short term signs indicating an upcoming construction of a new building or expansion of an existing building may not exceed 32 square feet. These signs may remain for a maximum of one year and shall be removed within 30 days of the building occupancy. These signs are not required to be located on a frontage.

(Section 8.080.K.5 added by Ordinance 04-04, 5-3-04)

6. Permits. Temporary banner style signs and lawn signs shall not require a sign permit. All other temporary or short term signs affixed to a building or site require a permit and shall be subject to the requirements of Section 8.060.

(Section 8.080.K.6 added by Ordinance 04-04, 5-3-04)

L. Portable Signs.

1. Exemption.
 - a. Portable Signs which are located within a street right-of-way shall comply with City Code Section 5.060, "Obstructions in Passageways", and are exempt from the requirements of the Sign Code in Astoria Development Code Article 8.
 - b. Temporary Portable Signs which are in conjunction with a community-wide event or festival such as, but not limited to, the Scandinavian

RESPONSES



→ No portable signs are proposed.



Festival or Regatta are exempt from the requirements of the Sign Code in Astoria Development Code Article 8.

2. Standards. All portable signs, except those exempted in subsection (1) above, shall be in accordance with the following:
 - a. Location. The sign shall not be located within a street right-of-way.
 - b. Number of Signs. Only one non-exempt Portable Sign shall be allowed for each business, use, or activity. Sites without buildings shall be allowed no more than one non-exempt Portable Sign. The sign shall not be included in the total number of signs allowed.
 - c. Area.
 - 1) A sign of eight (8) square feet or less on each side with a maximum height not to exceed four (4) feet and a maximum width not to exceed two (2) feet shall be exempt from the total square footage of sign area allowed.
 - 2) A sign exceeding eight (8) square feet on each side or a height greater than four (4) feet or a maximum width greater than two (2) feet shall be included in the total square footage of sign area allowed for the site.
 - d. Frontage. The sign may be located on any elevation of a business, use, activity, or site and is exempt from the requirement that signs be located on a frontage.
 - e. Lighting. The sign shall not be electric nor internally illuminated.
 - f. Enforcement. In addition to any other method of enforcement available to the City, this ordinance may be enforced by the removal of the sign by the City. The City will notify the permittee to reclaim the sign. If the sign is not claimed within 30 days after the permittee is notified, the City may dispose of the sign.

(Section 8.080.L added by Ordinance 00-11, 12-4-00)

M. Changeable Text Signs.

1. Exception.

Electronic signs that display time and temperature only are exempt from the requirements of Section 8.080(M).

RESPONSES



 No changeable text signs are proposed.



e. Operation of Sign.

- 1) Each sign or message shall remain fixed for at least ten seconds. No sign shall contain animation.
- 2) When a message is changed, it shall be accomplished immediately and shall not scroll, move, or otherwise change gradually.
- 3) The sign shall contain a default design that will freeze the sign in one position if a malfunction occurs.

(Section 8.080.M added by Ordinance 04-04, 5-3-04)

N. Signs for Upper or Basement Floors.1. Street Frontage.

- a. Number. The total number of signs for a separate business, use or activity in an upper story or basement of a building with street frontage shall be the same as buildings at street level. An upper floor or basement may have one sign at street level identifying the upstairs or basement activity.
- b. Area. The total area of signs for an upper story or basement of a building with street frontage shall not exceed 20% of the lineal footage of the building facade occupied by the business, use or activity, or the maximum allowed by the zone, whichever is less. A street level sign shall not exceed four (4) square feet.

2. Without Street Frontage.

- a. Number. The total number of signs for a separate business, use or activity in an upper story or basement of a building without street frontage shall be one sign.
- b. Area. The total area of signs for an upper story or basement of a building without street frontage shall not exceed four (4) square feet.

(Section 8.080.N added by Ordinance 04-04, 5-3-04)

(Section 8.090 deleted by Ordinance 00-11, 12-4-00)

RESPONSES



→ No signs for upper or basement floors are proposed.



8.150. C-2, C-3, GI, S-1, S-2, A-1, A-2, A-2A, LS, HR, CA, HC, AH-HC, FA ZONE SIGN REGULATIONS.

For all uses and sites in the C-2 (Tourist Commercial), C-3 (General Commercial), S-1 (Marine Industrial Shorelands), S-2 (General Development Shorelands), A-1 (Aquatic One Development), A-2 (Aquatic Two Development), A-2A (Aquatic Two A Development), LS (Local Service), HR (Hospitality/Recreation), CA (Education/Research/Health Care Campus), HC (Health Care), AH-HC (Attached Housing - Health Care), and FA (Family Activity) Zones, the following types, numbers, sizes and features of signs are allowed. All allowed signs must also be in conformance with the sign regulations of Sections 8.070 through 8.080.

(Section 8.150 amended by Ordinance 04-04, 5-3-04)

A. Total Square Footage Permitted.

The total square footage of all signage associated with a business site, use, or activity shall not exceed 150 square feet, with no single sign exceeding 100 square feet.

B. Freestanding Signs.

1. Number. One (1) sign shall be permitted for each site devoted to a single business, building, use or activity.

(Section 8.150.B.1 amended by Ordinance 04-04, 5-3-04)

2. Area. Total sign area shall not exceed one (1) square foot of sign area for one (1) lineal foot of site frontage that is not already utilized by other signs on the site or attached to buildings. Freestanding signs are allowed up to a maximum of 100 square feet. Allowable area on sites without buildings shall not exceed 32 square feet.

(Section 8.150.B.2 amended by Ordinance 04-04, 5-3-04; amended by Ordinance 12-03, 1-3-12)

3. Height. The maximum height of a freestanding sign shall be 24 feet.
4. Business Complex Signs. Two or more businesses or activities which are part of a business complex, strip mall or similar shared premises are permitted to erect one (1) freestanding sign within the allowable limit described in 8.150(B.1). Each individual business or activity shall not exceed 24 square feet of signage.



RESPONSES

→ The monument and wall sign will not exceed this square footage requirement.

→ No freestanding signs are proposed.



C. Wall, Roof Mounted, or Projecting Signs.

1. Area. The total allowable area for all permanent signs attached to the building is determined as follows:
 - a. A wall, roof mounted, or projecting sign of one (1) square foot per lineal foot of building frontage is allowed.
 - b. Individual sign face area. The maximum size of an individual sign within the total allowable area limits is 100 square feet.

D. Number of Signs.

The number of signs within the total allowable area is limited to two (2) signs per building frontage.

E. Temporary Signs.

One (1) temporary sign not exceeding 24 square feet is allowed.

F. Portable Signs.

Portable Signs are allowed in accordance with Development Code Sections 8.040 and 8.080.

(Section 8.150.F added by Ordinance 00-11, 12-4-00)

G. Changeable Text Sign.

1. Changeable text signs shall be allowed in the C-3 and S-2 Zones only and in accordance with Section 8.080(M).

(Section 8.150.G added by Ordinance 04-04, 5-3-04)

H. Monument Sign.

1. Number. One (1) sign shall be permitted for each site devoted to a single business, building, use or activity with a street frontage of up to 200 lineal feet. Lots with frontage in excess of 200 lineal feet may have a maximum of two (2) monument signs. Corner lots can count two (2) street frontages.
2. Area. Total sign area shall not exceed one (1) square foot of sign area for one (1) lineal foot of site frontage that is not already utilized by other signs on the site or attached to buildings. Monument signs are allowed up to a maximum of 100 square feet. Allowable area on sites without buildings shall not exceed 32 square feet.



RESPONSES

→ The frontage (or north elevation) is 101 feet. The wall mounted sign will not exceed 100 square feet per (b) below.

→ See answer to (a) above.

→ Only one sign is proposed.

→ No temporary signs proposed.

→ No portable signs proposed.



→ No changeable text signs proposed.

→ One monument sign is proposed.

→ The area of the sign will not exceed 100 square feet and will not in total when added to the wall sign size exceed the limit of 150 square feet.



(Section 8.150.H.2 amended by Ordinance 12-03, 1-3-12)

3. Height. The maximum height of a monument sign shall be 10 feet.
4. Business Complex Signs. Two or more businesses or activities which are part of a business complex, strip mall or similar shared premises are permitted to erect one (1) monument sign within the allowable limit described in 8.150(H). Each individual business or activity shall not exceed 24 square feet of signage.

(Section 8.150.H added by Ordinance 04-04, 5-3-04)

RESPONSES



→ The monument sign will not exceed 10 feet in height.

→ This section is not an applicable approval standard.



6 ARTICLE 14 - GATEWAY OVERLAY ZONE APPLICABLE APPROVAL CRITERIA

14.005. PURPOSE.

The purpose of the Gateway Overlay Zone is to implement the concepts and guidelines of the Astoria Gateway Master Plan, dated April 1997. The Gateway Overlay Zone is intended to be an intensively developed, mixed-use area which complements Downtown Astoria and the community as a whole.

14.010. DEFINITION.

As used in Article 14, unless the context requires otherwise, the following word shall have the meaning indicated:

SHOULD: A requirement, unless it can be shown that to comply with the requirement would be unreasonable, impractical, or unfeasible. Economic hardship alone shall not be justification for noncompliance with the requirement, but may be considered in conjunction with other reasons for noncompliance.

14.015. GENERAL PROVISIONS.

- A. In addition to conformance with the specific uses and standards of the individual zones, the following zones shall conform to the general regulations of the Gateway Overlay Zone in Sections 14.005 through 14.030.
1. Maritime Heritage (MH)
 2. Family Activities (FA)
 3. Attached Housing/Health Care (AH-HC)
 4. Health Care (HC)
 5. Education/Research/Health Care Campus (CA)
 6. Hospitality/Recreation (HR)
 7. Local Service (LS)
 8. Attached Housing (Mill Pond) (AH-MP)
 9. Civic Greenway Overlay Zone (CGO)
(Section 14.015.A.9 added by Ord 13-09, 10-6-14)



RESPONSES



This section is not an approval standard.



This section is not an approval standard.



The proposed development is zoned AH-HC.

10. Compact Residential Zone (CR)
(Section 14.015.A.10 added by Ord 13-09, 10-6-14)

(Section 14.015.A, map exhibit deleted by Ord 15-04, 6-15-14)

Gateway Overlay Zones (see Land Use and Zoning Map for actual zone boundaries)

B. Design Review.

Each public or private development proposal within the Gateway Overlay Zone will be reviewed for consistency with the Design Review Guidelines in Sections 14.020 through 14.030.

14.020. APPLICABILITY OF DESIGN REVIEW GUIDELINES.

The Design Review Guidelines shall apply to all new construction or major renovation. For the purposes of this Code Section, "major renovation" is defined as construction valued at 25% or more of the assessed value of the existing structure. The guidelines are intended to provide fundamental principles that will assist in the review of the proposed development. The principles identify both "encouraged" and "discouraged" architectural elements. They are broad design objectives and are not to be construed as prescriptive standards.

14.025. DESIGN REVIEW GUIDELINES.

A. Purpose.

These guidelines promote architectural elements that unify the Gateway Area by encouraging styles characteristic of Astoria. The historic architecture of Astoria is represented by a variety of styles. Differences in details may be seen from one neighborhood to the next. These guidelines advocate the simplicity of design which is characteristic of Uppertown and the working waterfront. Building styles and details not inspired by Astoria's past will be discouraged. Monotony of design should be avoided. Variety of detail, form, and siting should be used to provide visual interest.

The Gateway Plan encourages new construction to reflect historic building types found in the Uppertown area. Three historic building types commonly found in the area include waterfront industrial, commercial, and residential. These building types may be used as models for contemporary building design, but do not restrict or define their function.

The Guidelines make reference to, but do not require the use of, historic materials. Contemporary substitutions (i.e. composite materials), will not be discouraged if their texture, profile, and proportions are similar to those materials with historic precedent.



RESPONSES

→ This section is not an approval standard.

→ The proposed development is new construction.



→ This section is not an approval standard.



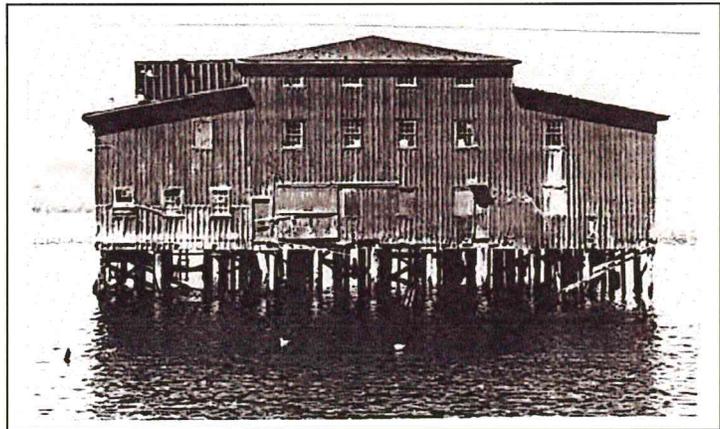
Figures included in Section 14.015 through 14.030 are included for illustrative purposes only and are not intended to be regulatory in and of themselves. If there is an inconsistency between the Figure and the Development Code text, the text shall prevail.

B. Building Forms Encouraged.

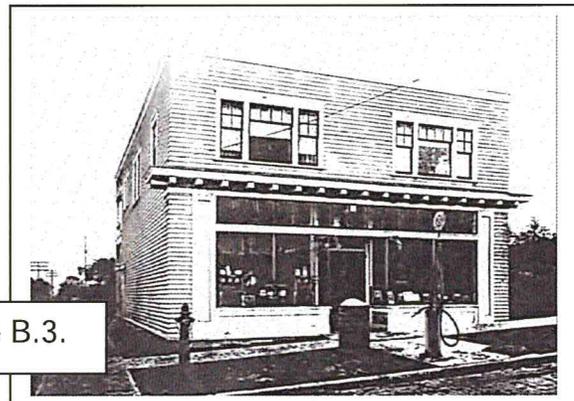
1. All Building Types.
 - a. Simple designs without extraneous details.
 - b. Rectangular in plan.
 - c. Square in plan.



2. Waterfront Industrial.
 - a. Low in form.
 - b. Cubic in form.



3. Commercial.
 - a. Low in form.



RESPONSES



→ The proposed building type is very simple with a general square plan shape. The square is offset slightly at the building entry corner to help identify the building entry.



→ The proposed development is not waterfront industrial.

→ The proposed development is commercial/medical office and is only two stories in height.



(Building Forms Encouraged)

4. Residential.
 - a. Vertical in form.
 - b. Cubic in form.
 - c. Full front porch or front porch large enough to accommodate several seated persons.

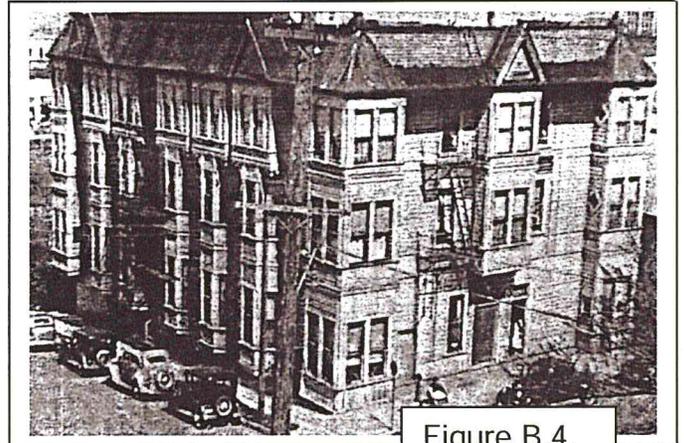


Figure B.4.

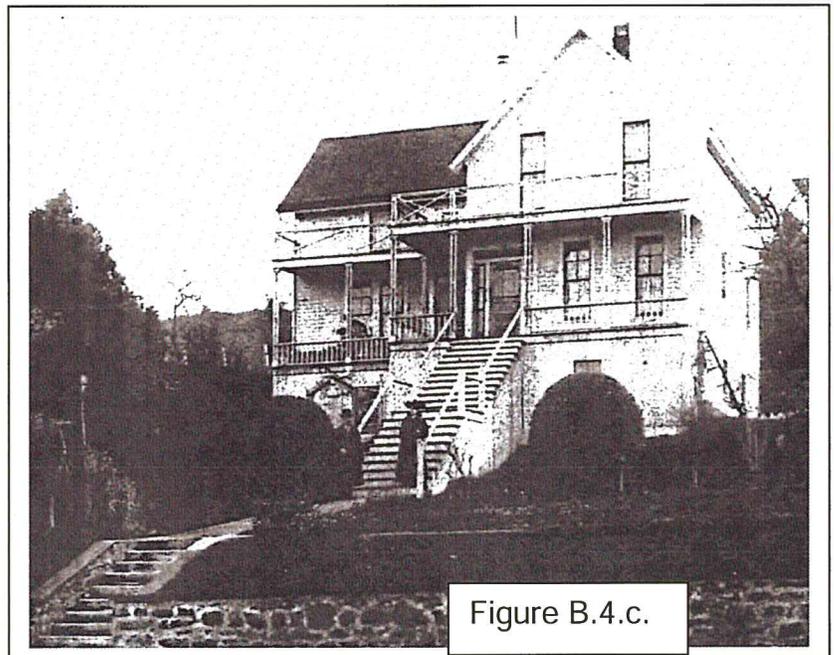


Figure B.4.c.

RESPONSES



→ The proposed development is not residential.



C. Building Forms Discouraged.

1. All Building Types.
 - a. Complex building footprints.
 - b. Sprawling structures.

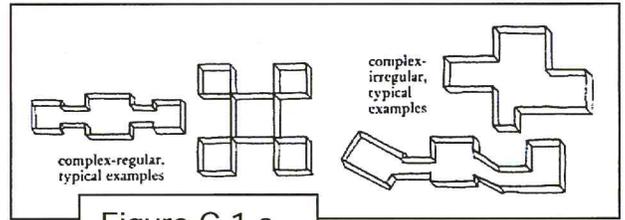


Figure C.1.a.

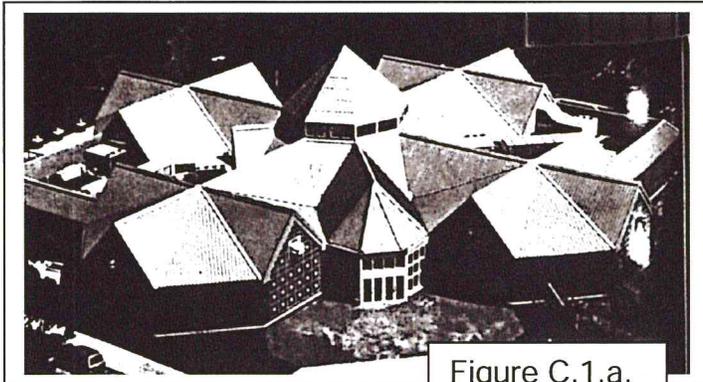


Figure C.1.a.

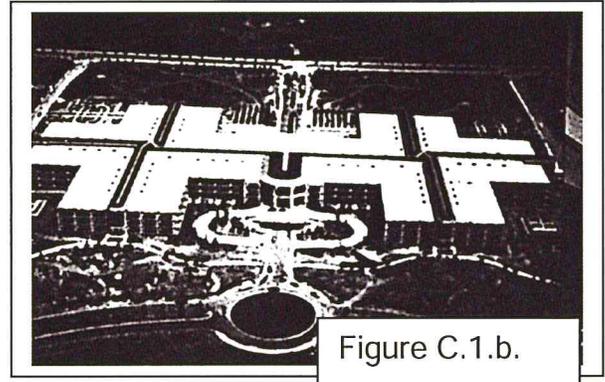


Figure C.1.b.

D. Windows Encouraged.

1. All Building Types.
 - a. True-divided, multiple-light windows.
 - b. Single-light windows.
 - c. Applied muntins with profile facing window exterior.
 - d. Rectangular windows with vertical proportions.
 - e. Fixed windows.
 - f. Double or single-hung windows.

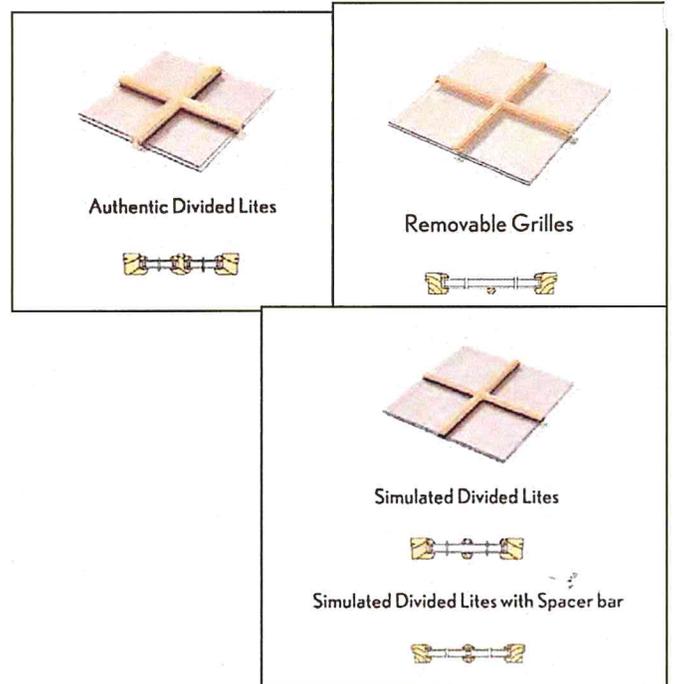


Figure D.1.c.

RESPONSES



→ The proposed development form is neither complex nor sprawling.



→ All window areas proposed are true-divided, multiple-light windows. Most window systems will be aluminum storefront with a possible curtain-wall system on the north elevation.



(Windows Encouraged)

- g. Casement windows.
- h. Windows should be spaced and sized so that wall area is not exceeded by window area, with the exception of commercial storefronts.

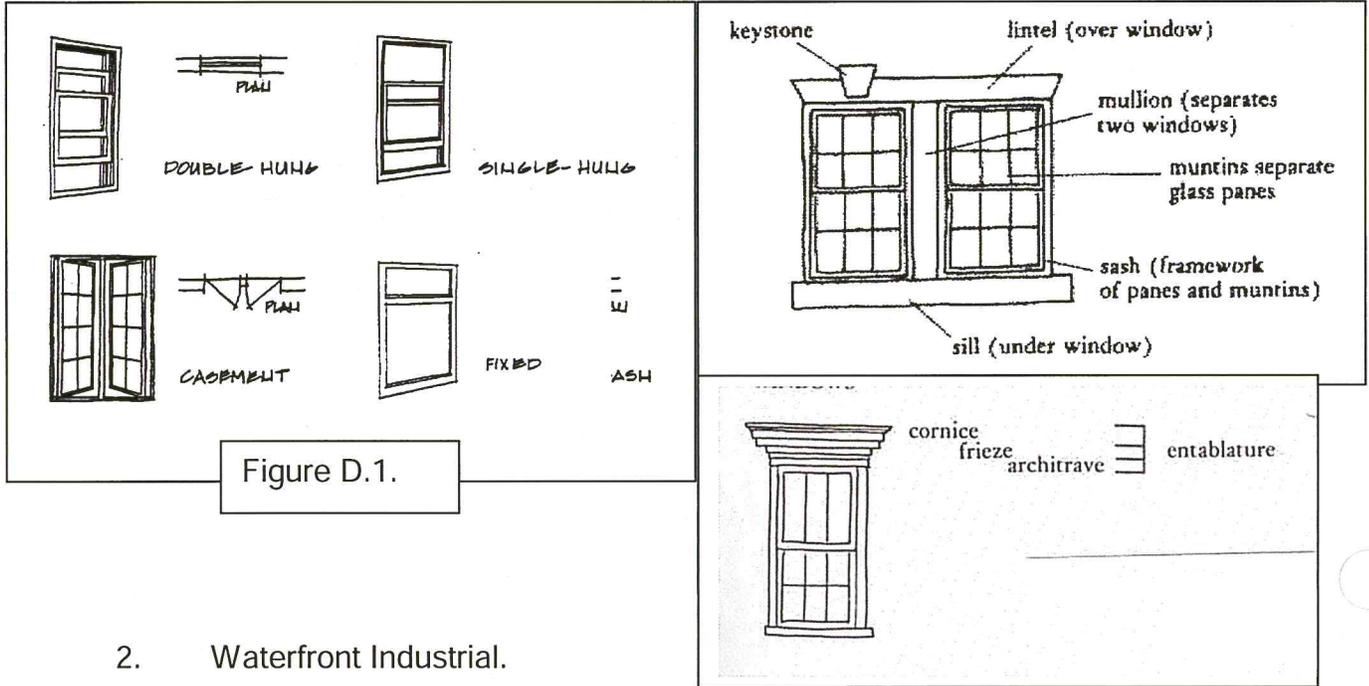


Figure D.1.

2. Waterfront Industrial.

- a. Square or rectangular windows with multiple lights.

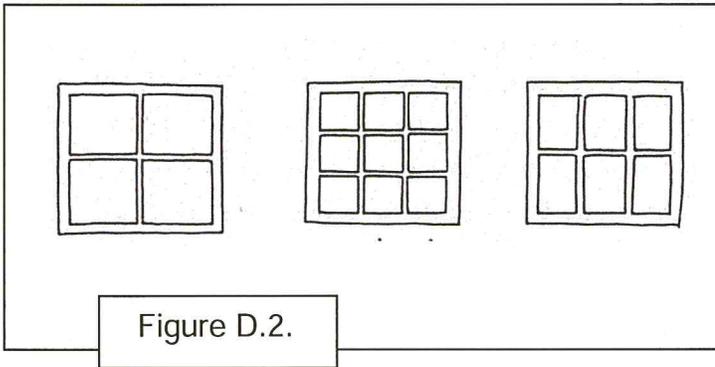


Figure D.2.

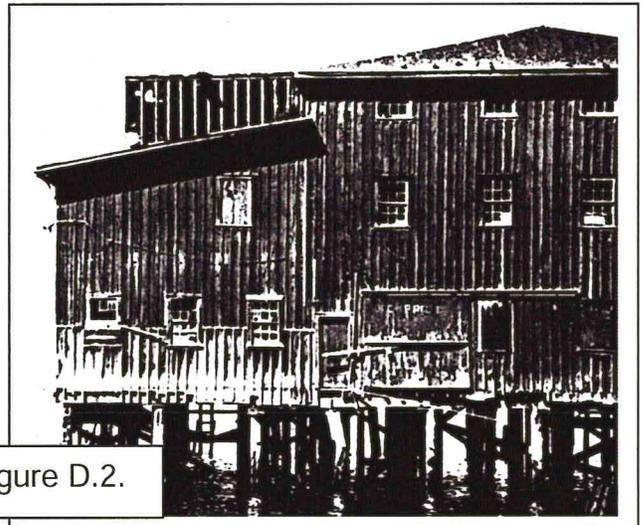


Figure D.2.

RESPONSES



→ There are large expanses of glazed envelope in this building to bring in natural daylight into the patient care spaces.



→ The proposed development is not waterfront industrial.

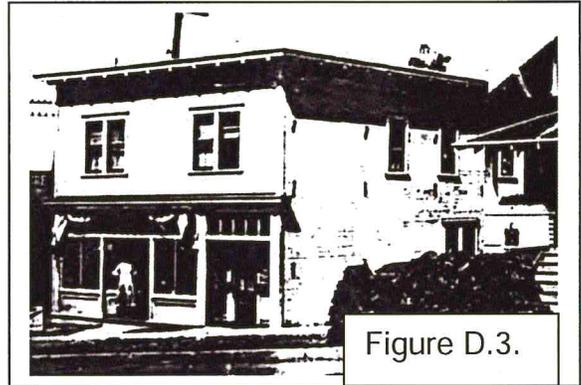


(Windows Encouraged)

3. Commercial.

a. Storefronts.

- 1) Plate glass windows with multiple-light transom windows above.
- 2) Recessed entries.
- 3) Window to wall surface proportions may be exceeded.



b. Upper Stories.

- 1) Window area should not exceed wall area.

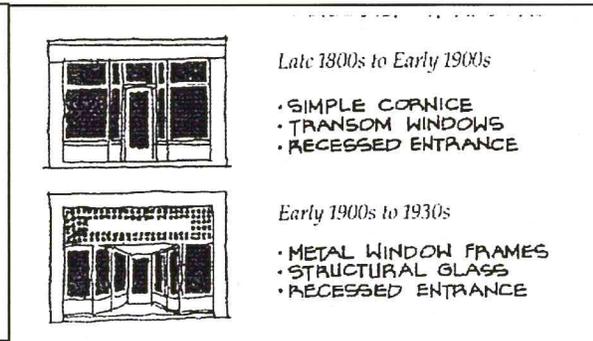
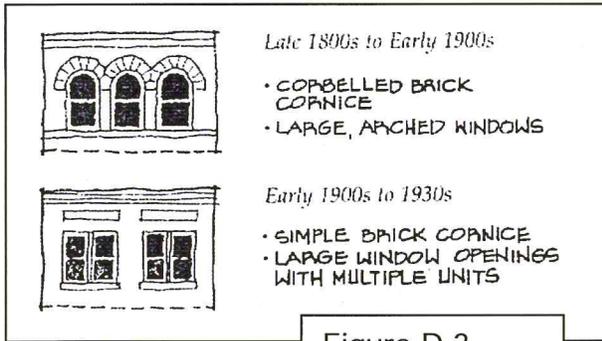


Figure D.3.

4. Residential.

- a. Vertical rectangle or square windows.
- b. Combination of single and multiple-light windows.
- c. Single windows, paired windows, or windows grouped in threes.
- d. Bay windows.

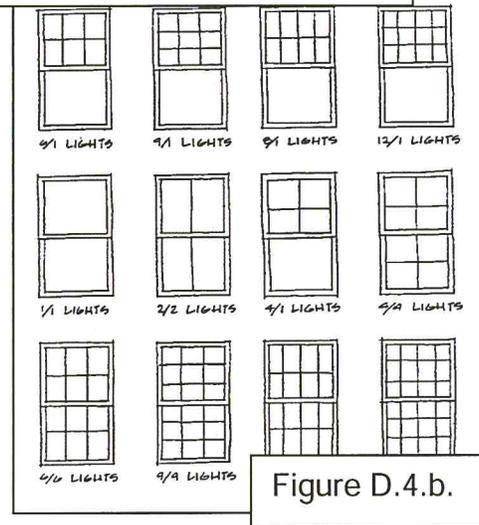


Figure D.4.b.



RESPONSES



Surrounding the building entry and waiting area for patients is a large storefront system glazed area. The second floor north elevation is the area of the program where medical oncology patients will be receiving multi-hour infusion treatments and will have the opportunity to view the river and scenic views to the north through that glazing system. This will be one of the highlights of the building.



The proposed development is not residential.

(Windows Encouraged)

- e. Arched or decorative shaped windows used sparingly.
- f. Windows should use casings and crown moldings.

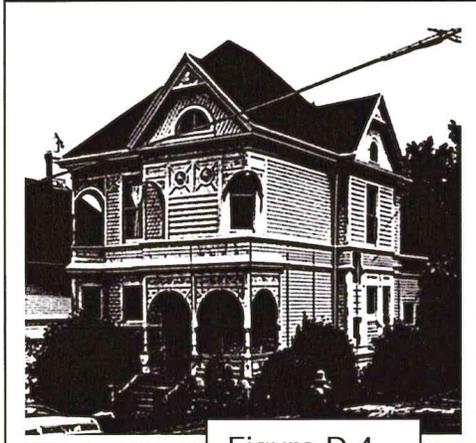


Figure D.4.

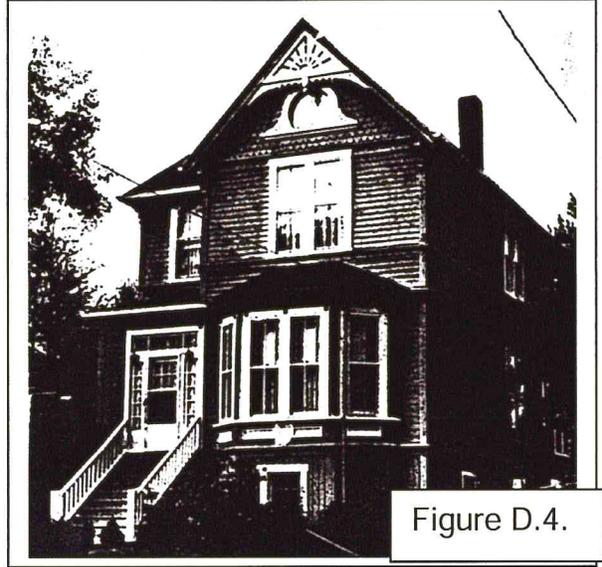
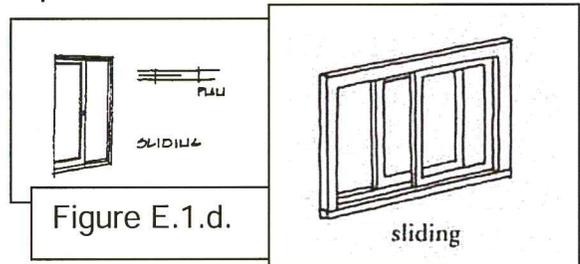


Figure D.4.

E. Windows Discouraged.

1. All Building Types.

- a. Applied muntins which have no profile.
- b. Smoked glass.
- c. Mirrored glass.
- d. Horizontal sliding windows.



- e. Walls predominated by large expanses of glass, except in commercial storefronts.
- f. Windowless walls. Large expanses of blank walls should only be located in areas which are not visible to the public.
- g. Aluminum frame windows, except in commercial storefronts.

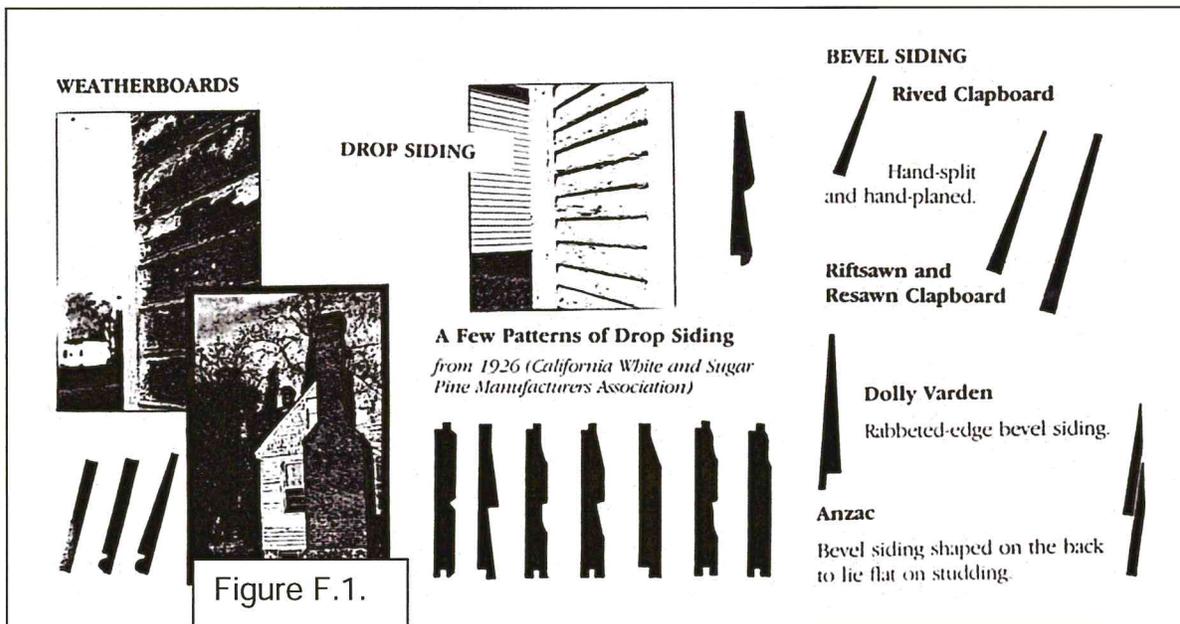
RESPONSES

None of the discouraged window types listed in a-g are a part of the proposed development. For the glazing systems around the front entry and the second floor medical oncology space, there may be some efforts taken in selecting a glazing system that will reduce glare and provide some level of privacy for patients. For example, in the north elevation on the second floor, where patients will be receiving multi-hour chemotherapy treatments, we want to maximize the natural light and view from that space to aid in the healing environment. At the same time, we need to be cognizant of the privacy of the view from the street and the glare from such a large expanse of windows. No smoked or mirrored glazing would be selected but there would be a slight reflectivity or low-e glazing system chosen for proper performance in this area.

F. Exterior Wall Treatments Encouraged.

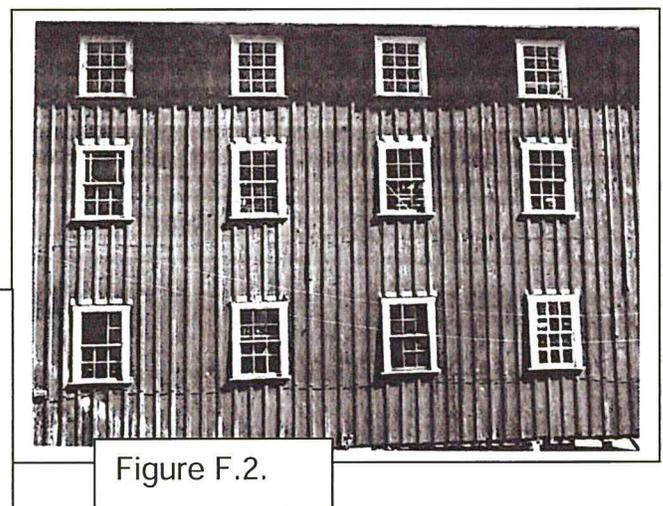
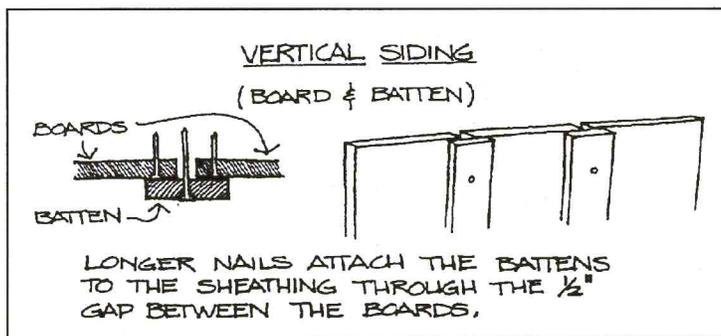
1. All Building Types.

- a. Drop siding.
- b. Weatherboard siding.
- c. Horizontal siding with six inches or less exposure.



2. Waterfront Industrial.

- a. Board and batten style.
- b. Galvanized corrugated metal.





RESPONSES



The exterior wall siding scheme will match in principle Columbia Memorial Hospital's Pavilion building. A combination of vertical board and batten siding, horizontal lap siding, galvanized metal panel, and some exposed concrete.



The proposed development is not waterfront industrial though the proposed development does take some queues from this building type.

(Exterior Wall Treatments Encouraged)

- 3. Commercial.
 - a. Finished concrete.
 - b. Brick veneer.

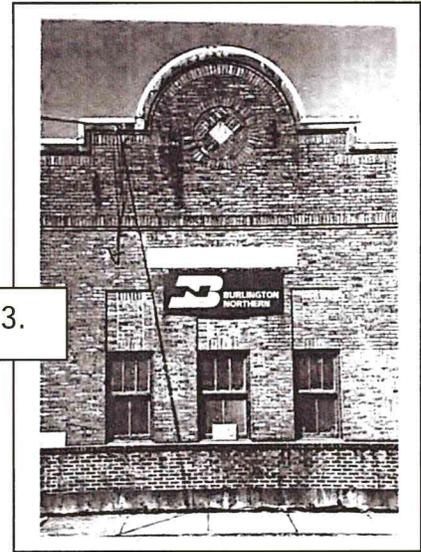


Figure F.3.

- 4. Residential.
 - a. Clapboard (see Figure F.1.)
 - b. Wood shingle (rectangular).
 - c. Decorative wood shingle.

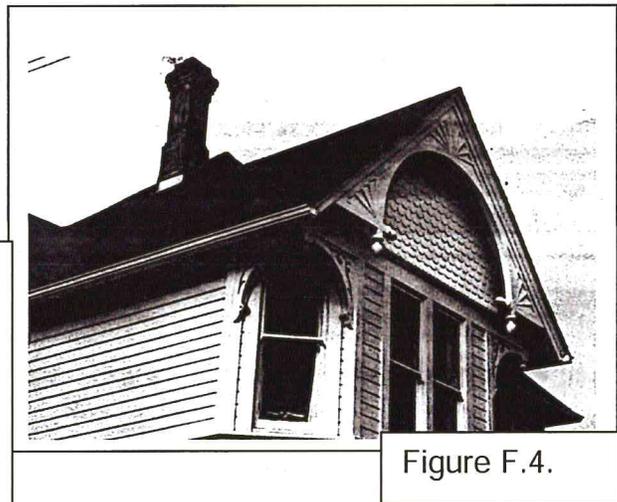


Figure F.4.

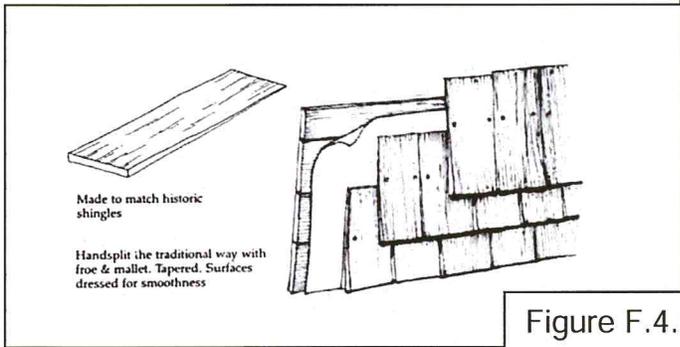


Figure F.4.b.



RESPONSES



The exterior wall siding scheme will match in principle Columbia Memorial Hospital's Pavilion building. A combination of vertical board and batten siding, horizontal lap siding, galvanized metal panel, and some exposed concrete.



The proposed development is not residential.



G. Exterior Wall Treatments Discouraged.

1. All Building Types.
 - a. Exposed textured, concrete block.
 - b. Flagstone or other applied stone products.
 - c. Precast concrete or decorative concrete panels.
 - d. Wood shakes.
 - e. Plywood paneling.

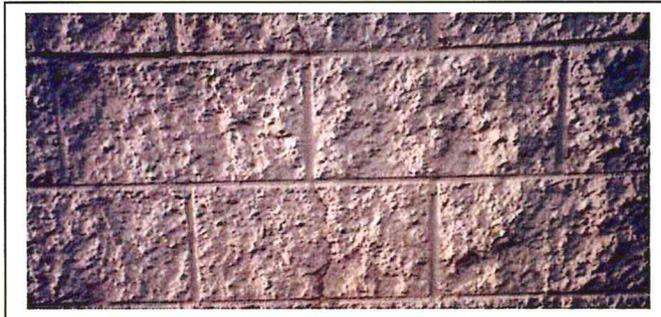
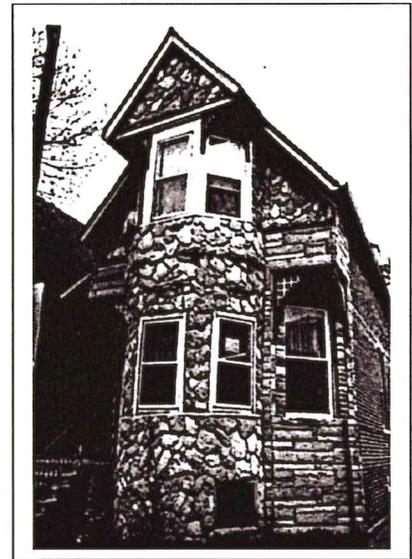


Figure G.1.a.

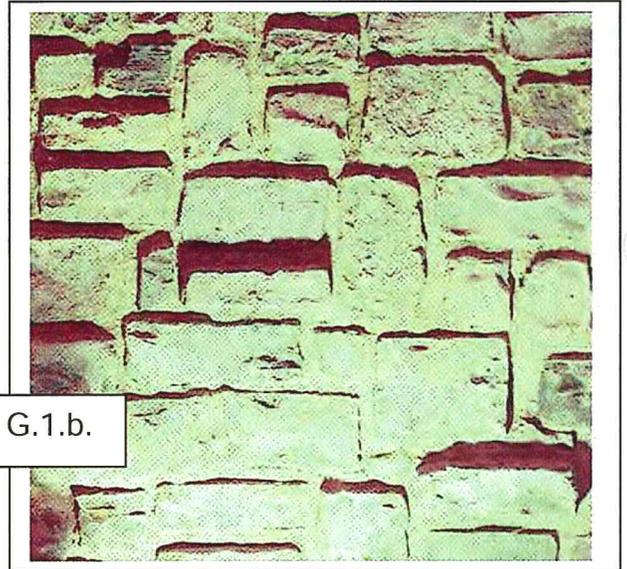


Figure G.1.b.

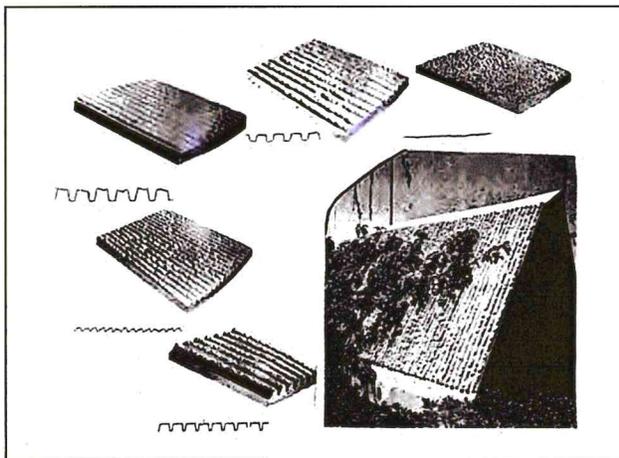
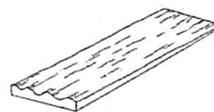


Figure G.1.c.



Typically:
 L = 15", 18", 24"
 W = 4" - 14"
 Butts vary
 mediums = 3/8-3/4"
 heavies = 3/4-1 1/4"

Commercially available. Machine split and sawn on the backs to taper. Split faces often irregular, even corrugated in appearance. Butt thickness vary and may be too wide.

Figure G.1.d.



RESPONSES

→ None of the discouraged exterior wall treatments listed in a-e are a part of the proposed development.



H. Roof Elements Encouraged.

1. Waterfront Industrial.

- a. Single gable with low pitch.
- b. Repetitive gable with steep pitch.
- c. Shallow eaves.
- d. Small shed roof dormers.
- e. Monitor roof on ridge line.
- f. Flat panel skylights or roof window.

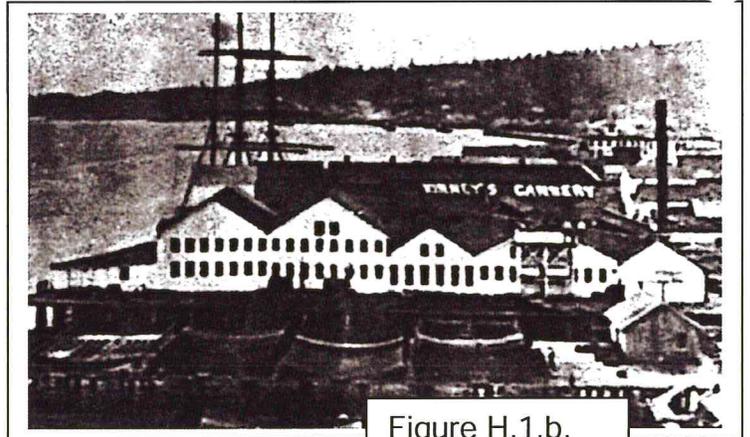


Figure H.1.b.

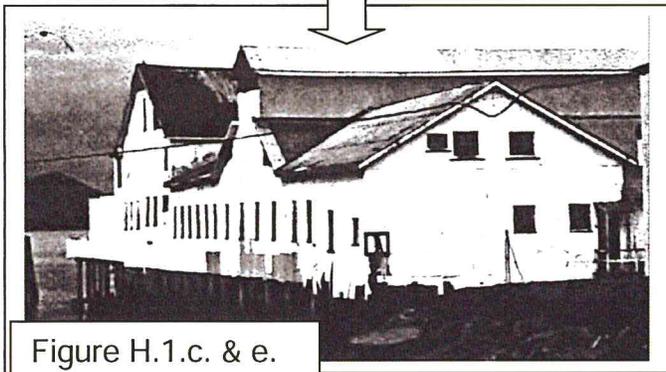


Figure H.1.c. & e.

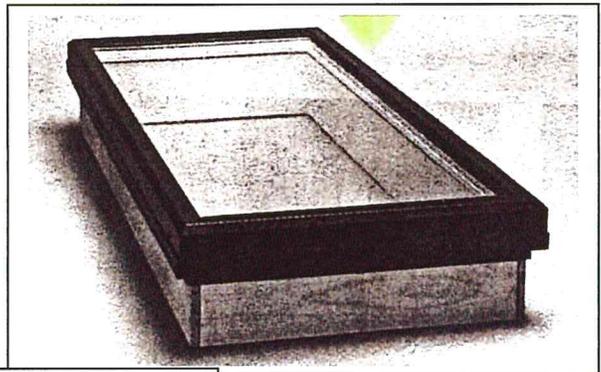


Figure H.1.f.

2. Commercial.

- a. Single gable with low pitch.
- b. Repetitive gable with steep pitch.
- c. Shallow eaves behind parapet wall.
- d. Flat or gable roof behind parapet wall.
- e. Structural skylights.

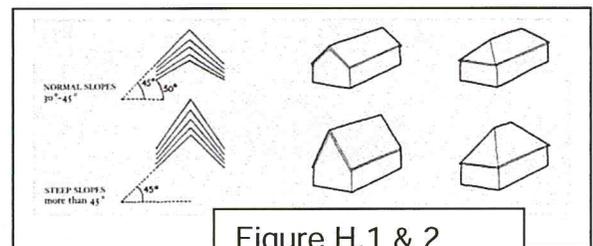


Figure H.1 & 2

RESPONSES



→ The proposed development is not waterfront industrial.



→ The roof system for the proposed development matches the roof type for Columbia Memorial Hospital's Pavilion building. It is a low slope barrel vault. This project will also have some flat roof elements with a parapet wall.



(Roof Elements Encouraged)

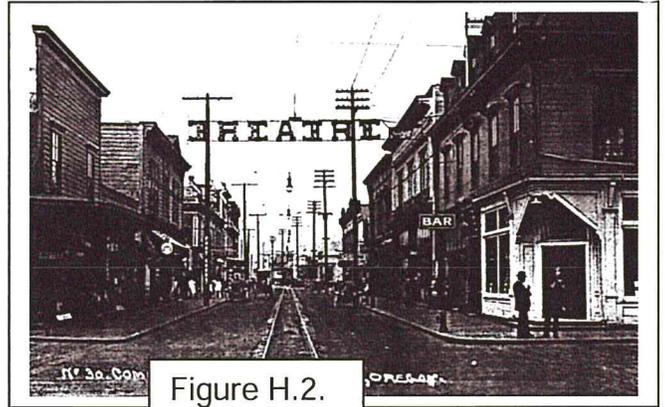
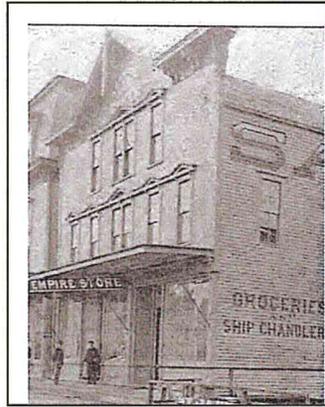
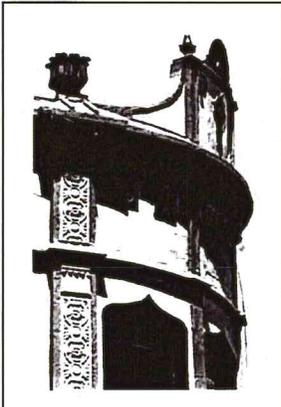


Figure H.2.

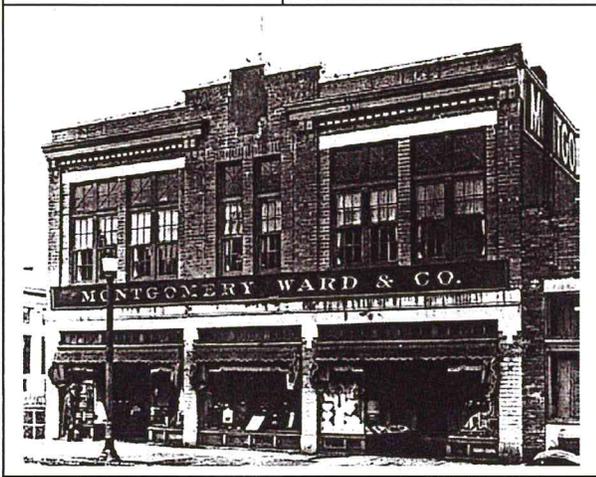


Figure H.2.c. & d.

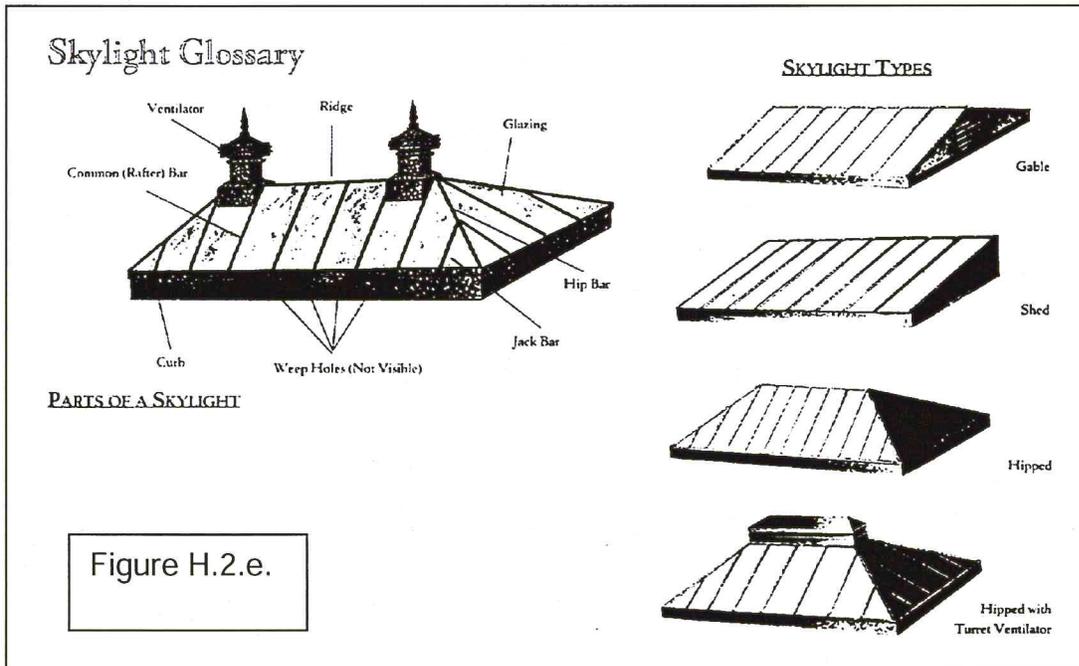


Figure H.2.e.

(Roof Elements Encouraged)

3. Residential.

- a. Steep gable with broad eaves.
- b. Steep hip with broad eaves.
- c. Dormers with gable, hip, or shed roofs.
- d. Flat panel skylights or roof window on secondary elevations.
- e. Turrets or large projecting window bays used sparingly.

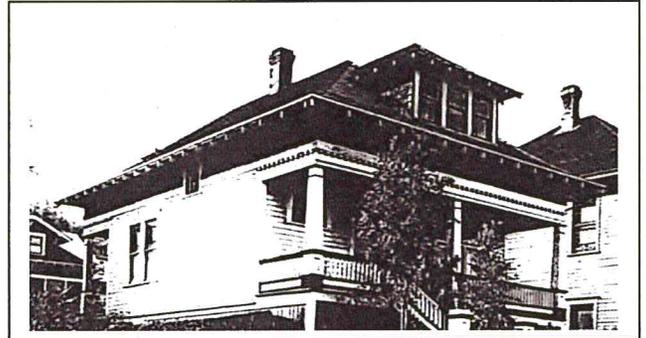


Figure H.3.b.

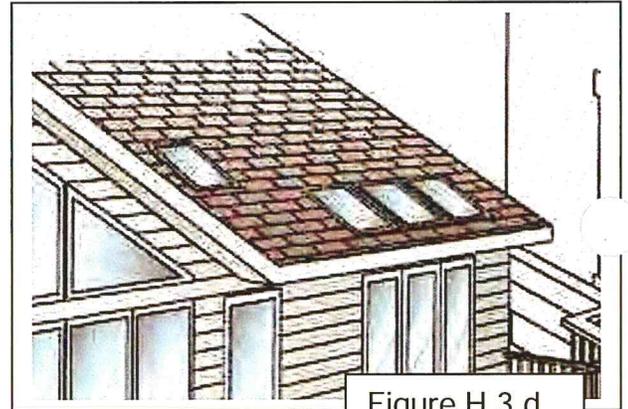


Figure H.3.d.

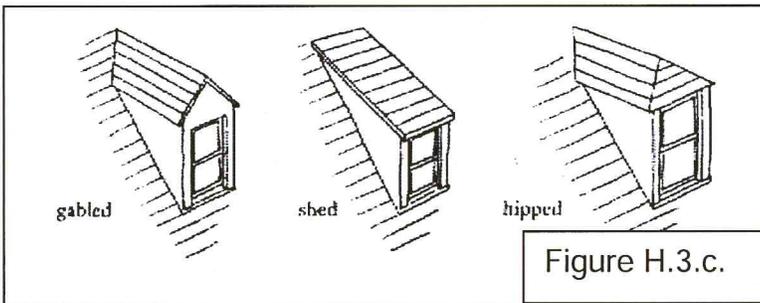


Figure H.3.c.

I. Roof Elements Discouraged.

1. All Building Types.

- a. False mansard or other applied forms.
- b. Dome skylights.

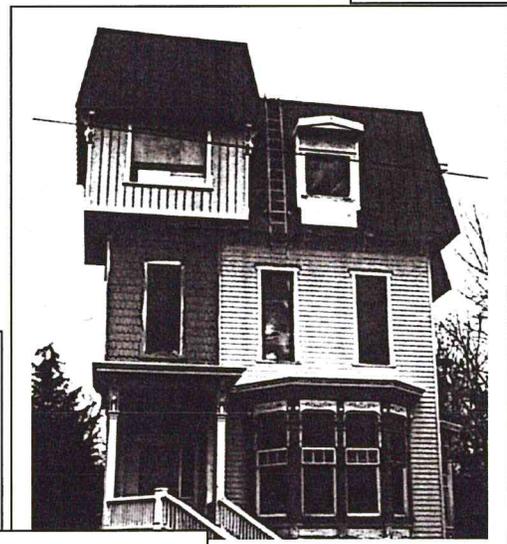
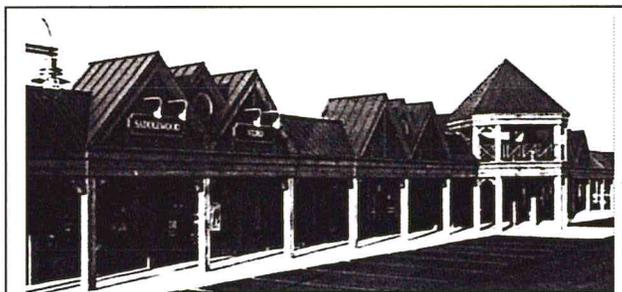


Figure I.1.

RESPONSES



→ The proposed development is not residential.

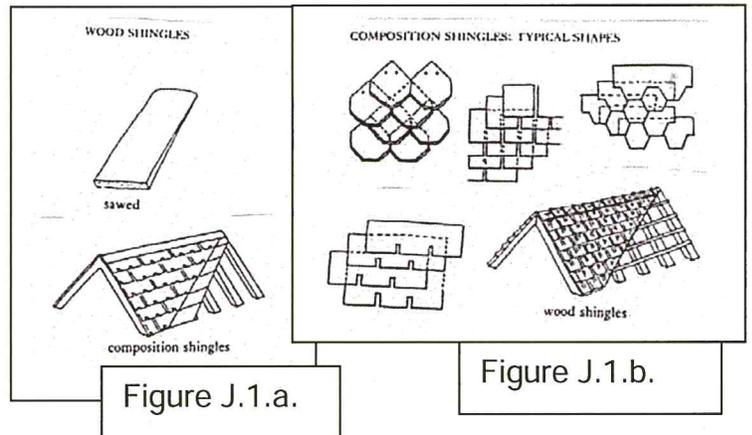


→ The roof system for the proposed development does not contain any of the discouraged elements.



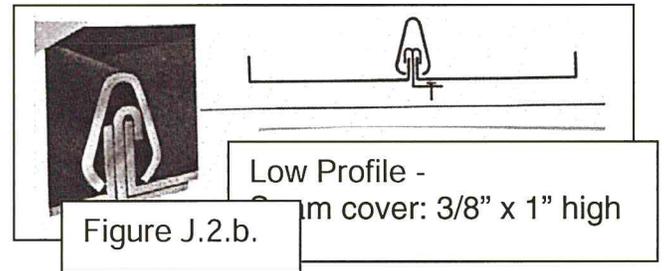
J. Roofing Materials Encouraged.

1. All Building Types.
 - a. Cedar shingle.
 - b. Composition roofing.
 - c. Roofing material in gray, brown, black, deep red, or other subdued colors.



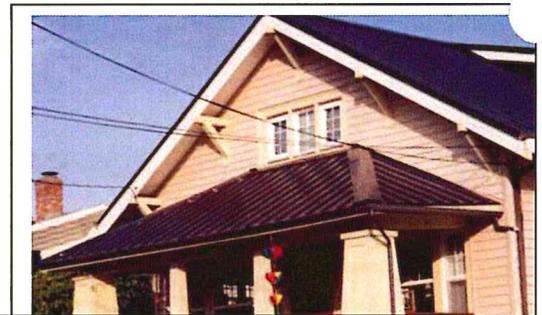
2. Waterfront Industrial.

- a. Galvanized corrugated metal.
- b. Low profile standing seam, metal roof.
- c. Roll down.



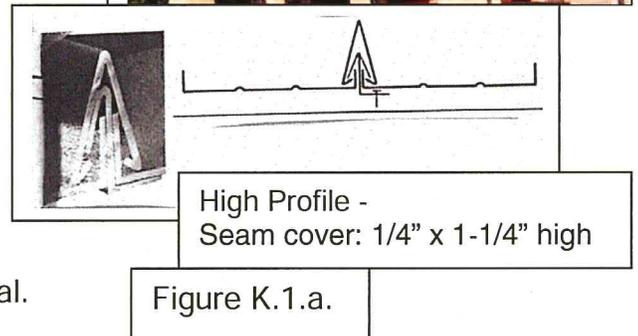
3. Commercial.

- a. Built-up.



K. Roofing Materials Discouraged.

1. All Building Types.
 - a. High profile standing seam, metal roof.
 - b. Brightly colored roofing material.





RESPONSES



The roofing materials will be a low profile standing seam metal in silver grey similar to the Pavilion building for the barrel vault and black EPDM membrane on the flat sections.



The proposed development is not waterfront industrial though the proposed development does take some queues from this building type.



The roofing materials will be a low profile standing seam metal in silver grey similar to the Pavilion building for the barrel vault and black EPDM membrane on the flat sections.



The roof system for the proposed development does not contain any of the discouraged elements.

L. Signs Encouraged.

1. All Building Types.
 - a. Hanging blade signs.
 - b. Signs painted on building facade.
 - c. Signs applied to building facade.
 - d. Front lit.
 - e. Graphics historic in character.

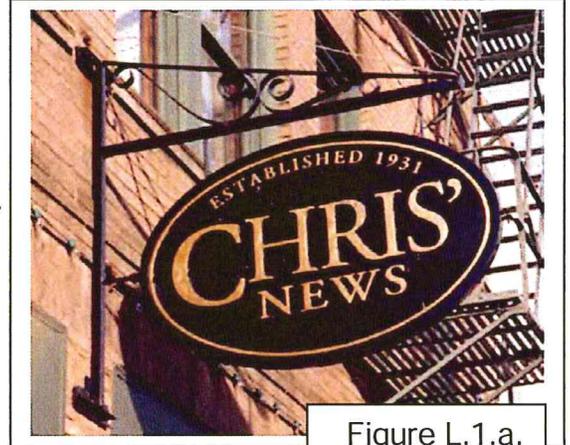


Figure L.1.a.

2. Commercial.
 - a. Exterior neon.



M. Signs Discouraged.

1. All Building Types.
 - a. Pole mounted freestanding signs.
 - b. Plastic or internal and back lit plastic.



Figure M.1.b.

N. Exterior Lighting Encouraged.

1. All Building Types.
 - a. Decorative lighting integrated with architecture.
 - b. Metal halide or incandescent.

RESPONSES



→ The building sign will be applied to the building facade.

→ The building sign will not be lit as it will face south, away from public view and the building is only accessed during daylight hours.



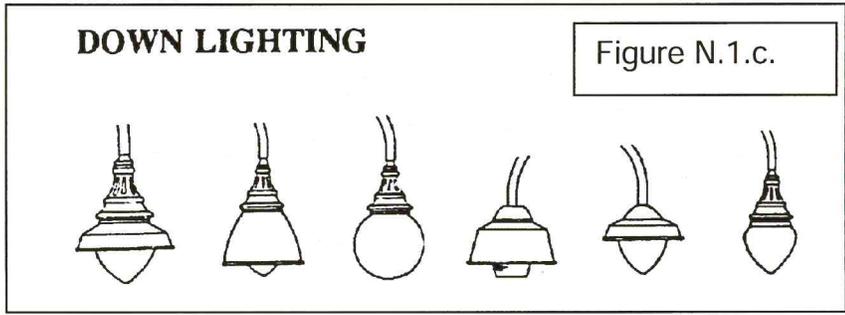
→ No discouraged signage is proposed.

→ Exterior decorative and parking lot lighting will be part of the project. Exterior lighting will face downward.



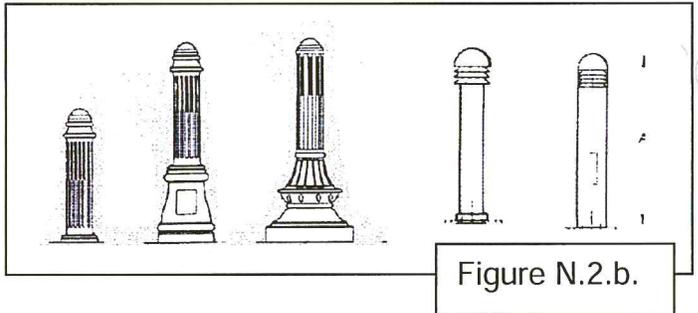
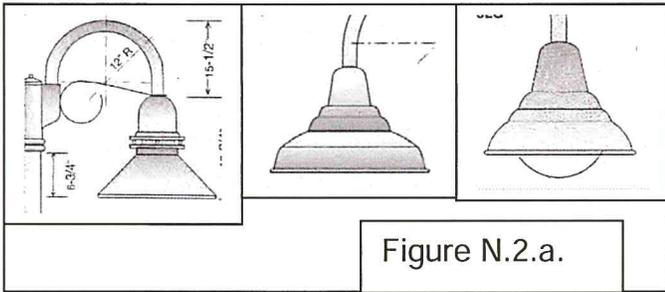
(Exterior Lighting Encouraged)

- c. Pedestrian and traffic signals combined with street lamps.
- d. Light fixtures that direct light downward and eliminate glare.



2. Waterfront Industrial.

- a. Industrial pan light with goose neck.
- b. Low bollard lighting.

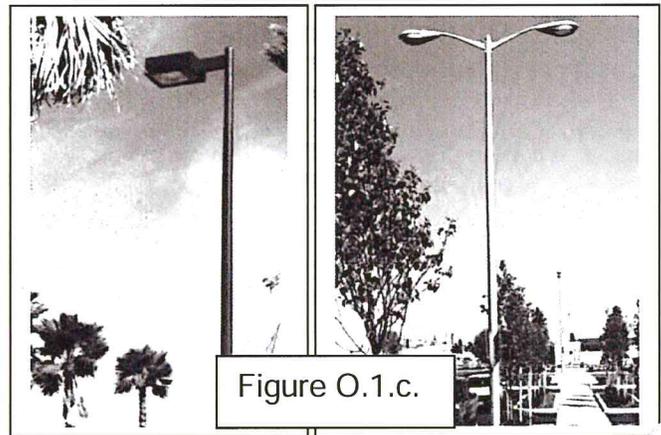


3. Commercial.

- a. Historic street lamps along walks and parking lots.

O. Exterior Lighting Discouraged.

- 1. All Building Types.
 - a. Sodium vapor (amber).
 - b. Fluorescent tube.
 - c. Cobra head street lamps or other contemporary fixtures.





RESPONSES

→ The proposed development is not waterfront industrial



→ The parking lot lighting will match the parking lot lighting in the new lot that was just completed in the fall of 2015.

→ The exterior lighting for the proposed development does not contain any of the discouraged elements.



(Exterior Lighting Discouraged)

- d. Fixtures with undifused, undirected light that do not focus the light to the ground and that will potentially destroy the night sky view.

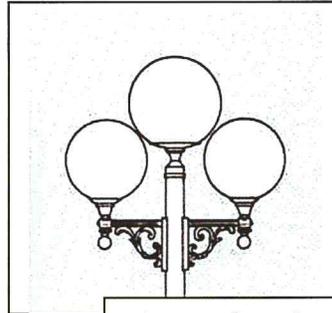
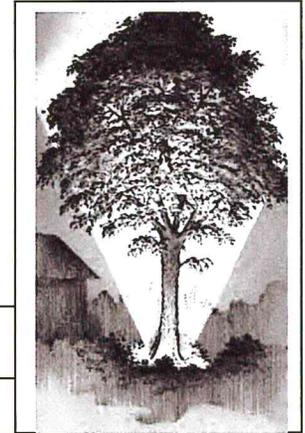


Figure O.1.d.



P. Other Design Elements Encouraged.

1. Commercial.

- a. Canvas awnings or fixed canopies for rain protection.

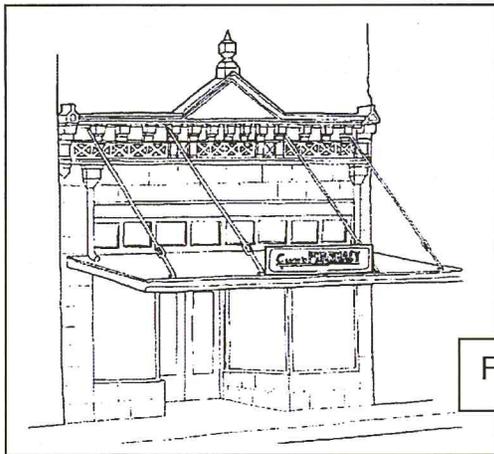
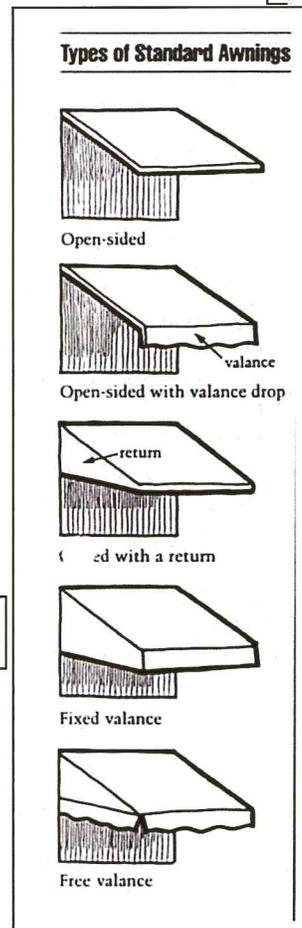


Figure P.1.





RESPONSES



The proposed development will have a large awning at the front entry for patient drop-off undercover, and awning over the entry to the survivorship garden and an awning over the west staff entry and access to the mobile PET/CT machine.



Q. Other Design Elements Discouraged.

1. Commercial.

- a. Vinyl awnings.
- b. Back lit awnings.

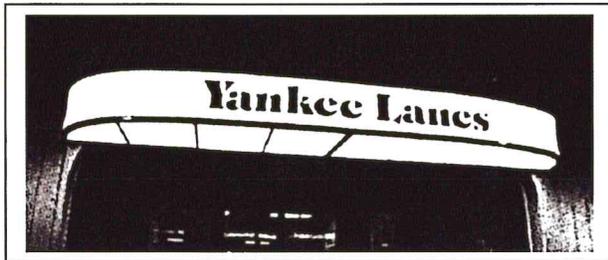
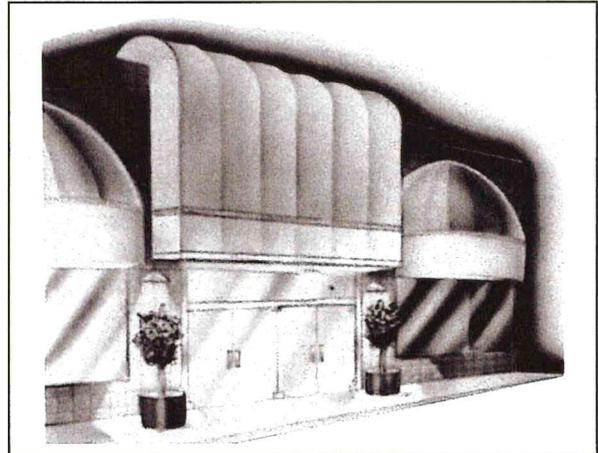


Figure Q.1.



RESPONSES



→ No vinyl or back lit awnings are proposed.



14.030. OTHER APPLICABLE USE STANDARDS.

A. Building Orientation.

1. Development projects should form visually continuous, pedestrian-oriented streetfronts with no vehicle use area between building faces and the street.
 - a. Exceptions to this requirement may be allowed to form an outdoor space such as a plaza, courtyard, patio, or garden between a building and a sidewalk. Such a larger front yard area should have landscaping, low walls, fencing, railings, a tree canopy, or other site improvements.
2. New uses should be sited to take advantage of the Columbia River and hillside views.
3. If the proposed project is large or situated so as to become an entrance or major focus of the City, the design should recognize the project's prominence and should be both compatible with its surroundings and complementary to the City as a whole.

B. Building Massing.

1. Buildings should have a floor area ratio on their lots of at least 1:1 (One square foot of building area for one square foot of lot area), in order to maximize use of the land.
2. Buildings should be a minimum of 24 feet in height from grade to highest point of the structure, excluding those features exempt from building height as identified in Development Code Section 3.075.
3. The height, mass, and scale of buildings should be compatible with the site and adjacent buildings. Use of materials should promote harmony with surrounding historic structures and the character of the waterfront.

C. Access and Parking Design.

1. All uses which are served by an alley, local street, or collector street should have alley or street vehicular access and egress. Curb openings onto Marine Drive or Exchange Streets are discouraged. Parking lots should be on the interiors of blocks or behind buildings, and should be designed to be as unobtrusive as possible.
2. Building facades and entries should face the adjacent street. Main entrances should face a connecting walkway with a direct pedestrian connection to the



RESPONSES

→ The proposed development is positioned at the minimum front yard setback of 10' and provides a visually continuous street front with no vehicle use between the building face and the street.

→ No exception necessary for this development.

→ The development of the design and the siting for this project was heavily weighted on providing views of the Columbia River for the medical oncology patients. This is a paramount aspect to the design. Views to nature and the river is a key part of providing a healing environment for this patient population.

→ The proposed project meets the requirements of the GOZ.



→ The development site for this project covers multiple lots and a large portion of the area that was once John Warren football field. The proposed area of work encompasses an area of about 91,000 square feet. There is no medical use that requires 91,000 square feet of programmed space to meet a 1:1 requirement. This project has an FAR of 0.22.

→ The proposed building height is 31'-3" and does have features that are excluded from the building height calculation per 3.075.

→ The proposed project meets the requirements of the GOZ.

→ No alley is accessible from the proposed site. A curb opening is proposed for Exchange Street but meets intersection spacing requirements and is supported by the traffic engineer analysis found in Appendix A. The parking lot is behind the building.

→ Please see answer to this section on Page 144.



street without requiring pedestrians to walk through parking lots or across driveways.

3. Parking areas should be shared among various uses where a development or block is planned as a whole. On-street parking on internal streets may be counted towards the off-street parking requirements with the approval of the Community Development Director.

D. Landscaping.

1. Street trees should be planted within the right-of-way along both sides of the streets within the Gateway Overlay Zone.
 - a. Spacing should be 30 feet on center, depending on species and branching habit.
 - b. Minimum size of deciduous trees should be 2" caliper, with an upright form.
 - c. Mature branching height should be a minimum of 15'.
 - d. Durable tree grates and trunk protectors should be installed.
2. Areas between trees should be landscaped with a variety of shrubs and perennials, with an emphasis on flowering species.

E. Underground Utilities.

This provision shall apply only to utility lines to be installed for new construction. Utility lines, including, but not limited to, electricity, communications, street lighting and cable television, shall be required to be placed underground. Appurtenances and associated equipment such as surface-mounted transformers, pedestal-mounted terminal boxes and meter cabinets may be placed above the ground, and shall be screened by sight obscuring fences and/or dense landscape buffers. The Design Review Committee may waive the requirements of this section if topographical, soil, or other conditions make such underground installations or screening of above ground equipment unreasonable or impractical. The applicant shall make all necessary arrangements with the serving utility or agency for underground installations provided hereunder; all such installations shall be made in accordance with the tariff provisions of the utility, as prescribed by the State Public Utilities Commissioner.



RESPONSES



There are no shared uses for this proposed development and no plan to count on-street parking as part of the whole.



Per 14.010 - "SHOULD: A requirement, unless it can be shown that to comply with the requirement would be unreasonable, impractical, or unfeasible". In this case, it is important to note that the view from the second floor medical oncology space to the river is paramount to creating a healing environment for these patients dealing with cancer. The opportunity to watch the ships pass by and see the natural landscape is very important. Street trees in front of the building will block that view. We would like to request that this guideline is unreasonable per 14.010 and an accommodation is made to not develop street trees along the north elevation Exchange street frontage. If there is a plant or tree material that could have a capped height of 8 feet or less that could be an accommodation. The hospital intends on negotiating with the cinema owner on the north side of Exchange to remove their trees from the hillside in exchange for planting new shrubbery in and around their parking lot. This will help the view corridor as well.



The proposed development has been reviewed by City engineering and will comply with these requirements.



14.030(C)(2). Access and Parking Design.

2. Building facades and entries should face the adjacent street. Main entrances should face a connecting walkway with a direct pedestrian connection to the street without requiring pedestrians to walk through parking lots or across driveways.

RESPONSE: The response to guideline 14.030(C)(2) will be broken down into four main sections. The definition of the word "should" per Article 14.010 and the latitude the code provides for unreasonable or impractical reasons for noncompliance, the relevant precedent of buildings in the immediate vicinity, the medical use and patient population this building will serve, and finally references to other city development code in the state of Oregon.

I. Article 14.010

From section 14.010 - DEFINITION. As used in Article 14, unless the context requires otherwise, the following word shall have the meaning indicated: SHOULD: A requirement, unless it can be shown that to comply with the requirement would be unreasonable, impractical, or unfeasible. Economic hardship alone shall not be justification for noncompliance with the requirement, but may be considered in conjunction with other reasons for noncompliance.

As stated in this section, it is acceptable for noncompliance when it can be shown that to comply would be unreasonable or impractical. The applicant believes the proposed location for main building entry and secondary entry are appropriate for the site and the medical use of the building.

Let us first look closely at the parts of section 14.030(C)(2):

"Building facades and entries should face the adjacent street..." Taken at face value, this first sentence of the section presents the guideline that the building facade should face Exchange Street. This is unreasonable and impractical for the following reasons:

1. The topography of the site from Exchange down to where the finished floor elevation would be represents an approximate 5 foot drop. To enter directly from Exchange, a mid-floor entry would be required. Because of the patient population and medical use, all entries need to be ADA accessible. This would require the elevator to be located near this entry with a mid-floor third stopping point to get patients from the entry down to the first floor or up to the second floor.
2. The Astoria Fire Chief will require the entire building frontage to be red-striped to prevent on-street parking. No on-street parking will be allowed anywhere near an entry on Exchange. Given the patient population and fragile health of the users of this building, this entry would not be convenient, accessible, or safe.
3. In order for CMH to be successful with this project and provide state-of-the-art healthcare for the residents of Astoria, Clatsop County, and the north Oregon coast, it is imperative the project work financially. The program of this building is maximized in the smallest possible footprint required to reduce construction costs. A mid-floor entry point with three stop elevator and/or ADA ramping up and down would add a considerable amount of cost to the project.

The second sentence of this code section reads, "Main entrances should face a connecting walkway with a direct pedestrian connection to the street without requiring pedestrians to walk through parking lots or across driveways." This approval criteria is completely satisfied with the

proposed development:

1. The proposed main entry faces a connecting sidewalk with a direct pedestrian connection to Exchange Street. This is a 6 foot wide pedestrian path that does not require a pedestrian to cross a parking lot nor to cross a driveway to access the public street. This pathway is also at an ADA accessible slope. None of the precedent examples in the following section have this public amenity.

II. Entry Precedent in the Immediate Vicinity

There are four examples within two blocks along Exchange Street where this standard is not met or in practice.

Dental building along Exchange west of 18th

This building has an "entry" facing Exchange but it is not used as such in any way. As you can see, the garbage is put out in the pedestrian path leading up to it. It is locked and not marked as an entry, and behind the locked gate is where all of the garbage, recycling, and metering is located. The main building entry is on the south side of the building away from Exchange facing the parking lot. This building is a block from the proposed development.



Builders Supply

This operation has an entry to another building along 18th street, but this building, which is separate, has no entry on Exchange Street.



Cinema

This building is directly north of the proposed development between Exchange Street and Marine Drive. The building is fronted directly along Marine Drive. There is no entry facing Marine Drive or Exchange. The main entry faces east and the parking lot. There is no ADA accessible route from the pedestrian sidewalk on Exchange or Marine Drive to the building entry.



Aquatic Center

The aquatic center is also directly north of the proposed development, is between Exchange Street and Marine Drive, and has building frontage directly along Marine Drive. This building



does have an "entry" along Marine Drive but it is locked at all times. There are signs warning that you are being videotaped and signs directing you to the main building entry on the south side of the building which faces the parking lot. Therefore this building too has no ADA accessible entry from a pedestrian connection on either Exchange or Marine Drive.



III. Building Use and Patient Population.

It is vitally important to remember that the users of this building are clinical patients, usually geriatric or over 50 in age, who are dealing with various stages of cancer. This is NOT a retail building. There are no users of this building who will walk-in. ALL users of this building are sick or dealing with a sick loved one, and have regularly scheduled appointments, sometimes months in advance. An Exchange Street access point will not help any of the users of this building. With no on-street parking nearby, it would be a very unsafe and completely unused building feature.

We conducted a survey over the course of a week of the patients currently receiving chemotherapy treatments in the hospital's current cancer treatment facility in the Park medical building. Results:

1. How often do you walk or travel by bike to your appointment at our clinic?
 - a. Never - 29/31 (94%)
 - b. Rarely - 1/31 (3%)
 - c. Regularly - 1/31 (3%)

The overwhelming majority of patients do not walk or bike to their appointments leaning credence to the idea that a main entrance from the pedestrian sidewalk is not imperative. (Please remember that the proposed sidewalk from Exchange to the front entry is ADA accessible for the 2/31 patients who sometimes walk)

2. How often do you use public transportation (city bus) to travel to your appointment at our clinic?
 - a. Never - 30/31 (97%)
 - b. Rarely - 1/31 (3%)

An even greater majority of patients do not use public transportation for appointments and therefore do not need a pedestrian connection from a transit stop. (Please remember that the proposed sidewalk from Exchange to the front entry is ADA accessible for the 1/31 patients who rarely take public transit)

3. How often do you drive or are driven to your appointment at our clinic?
 - a. Always - 28/31 (90%)
 - b. Regularly - 3/31 (10%)

Again, this stat confirms that basically all patients either drive or are driven to the building for appointments. In this case, having a safe, on-grade path from the parking lot to the main entry is critical for the welfare of this patient population.

We also surveyed the staff about their habits around lunch and transport to work. 15/22 staff never walk or bike to lunch in the area and another 5/22 rarely do. Only 2 out of 22 staff leave the building regularly for lunch. Zero staff take public transportation during their lunch breaks. 20/22 staff also never or rarely drive off campus for lunch.

The break room is the dominant area in the building for lunch breaks and the grab-n-go food offered in the buildings resource library will also augment staff needs. Staff will occasionally walk to the hospital to have lunch in the cafeteria. This is easily conducted by the 6 foot ADA accessible walk from the building entry to the sidewalk on Exchange and over to the hospital.

IV. Precedent City Code

We reached out to planners in other communities with health centers in Corvallis and Salem. Both of those jurisdictions have planning codes and zoning designations for healthcare and hospital services. They reported that their hospital zone and overlays did not have any street entry requirements. They agreed that it was something that retail/commercial districts often want to regulate. As this buildings use is not retail or commercial, we feel this guideline is not an applicable approval standard.

7 FIGURES



Penthouse = Exempt per 3.075

Roof peak = 35' elevation above finish floor grade

Building height = 31'3"

Eve = 27'6"



Figure 2
SOUTH ELEVATION

NOTES

1. Horizontal lap siding
2. Mechanical Penthouse
3. Concrete siding (linear accelerator vault)
4. Board and batten siding (final color TBD)
5. Aluminum frame windows

6. Survivorship garden
7. Building wall sign
8. Vertical metal siding
9. Main entry
10. Aluminum storefront entry glazing
11. Covered drop-off awning

12. Wood canopy supports
13. ADA accessible walk to pedestrian sidewalk on Exchange Street



Figure 2
EAST ELEVATION

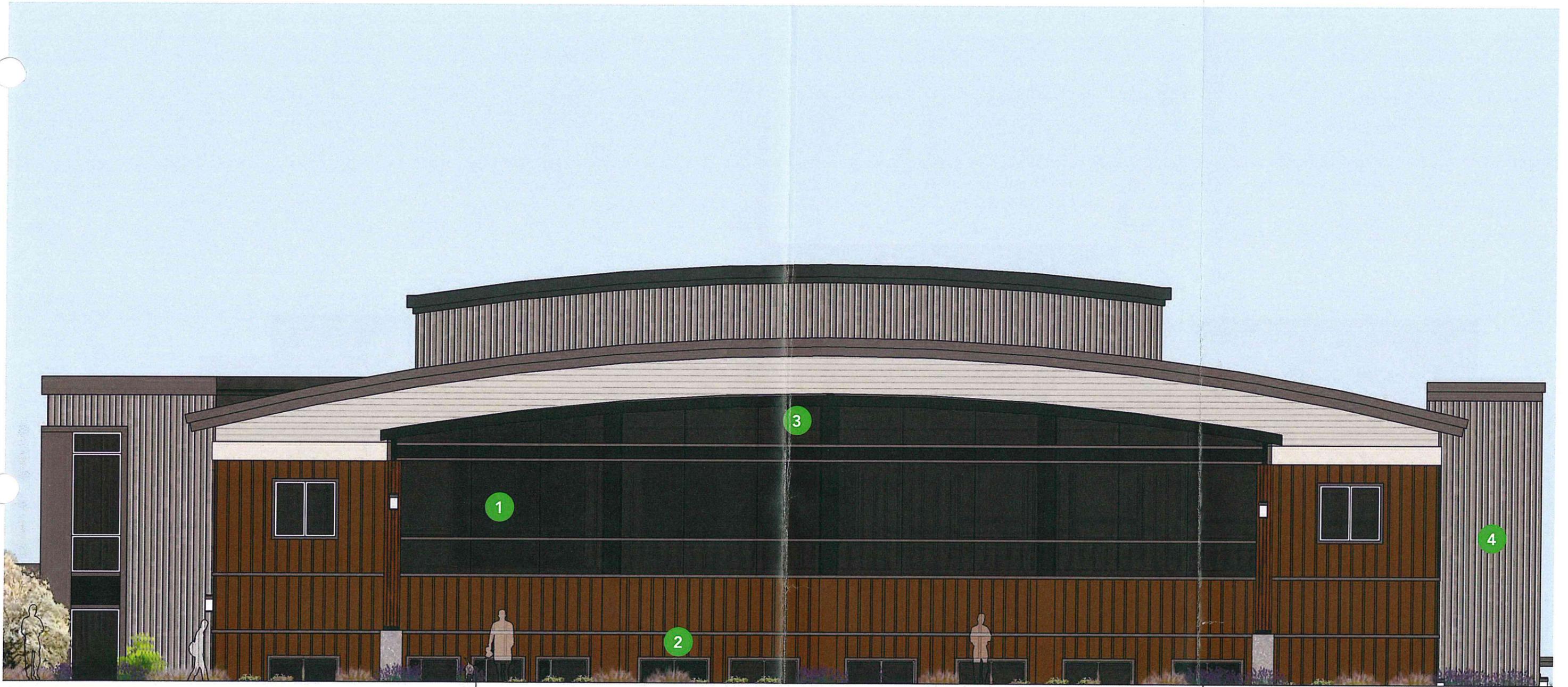


Figure 2
NORTH ELEVATION

NOTES

1. Primary view windows for patients receiving chemotherapy treatments
2. Offices and conference room windows on below Exchange grade first floor.
3. Downlighting from decorative metal eyebrow

- canopy.
4. West exit staircase.

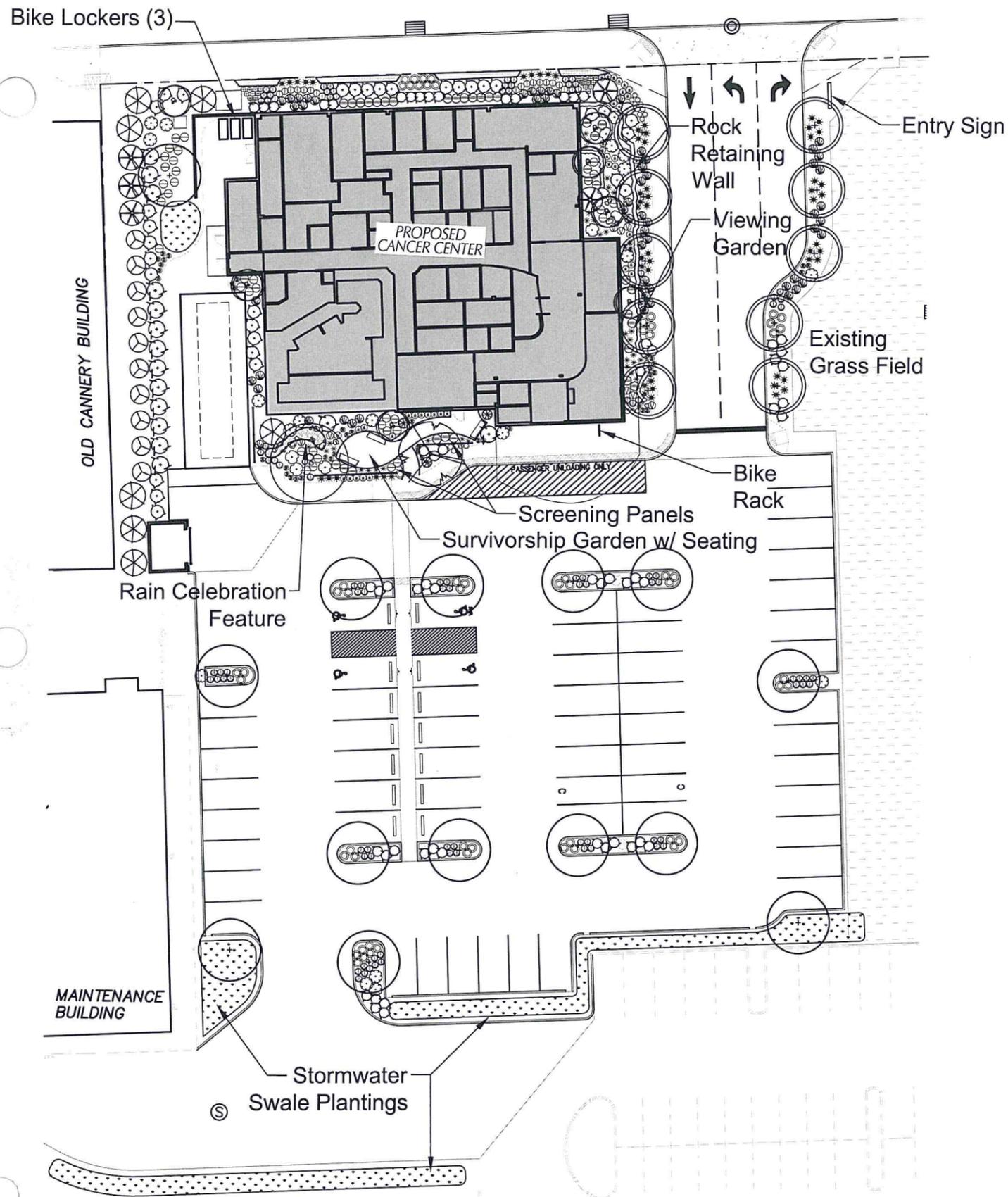


Figure 2
WEST ELEVATION

NOTES

1. Stair exit to outside.
2. Alternate staff entry, access to mobile PET/CT pad.
3. Physicist required concrete barrier for radiation from the linear accelerator machine.

4. Survivorship garden.

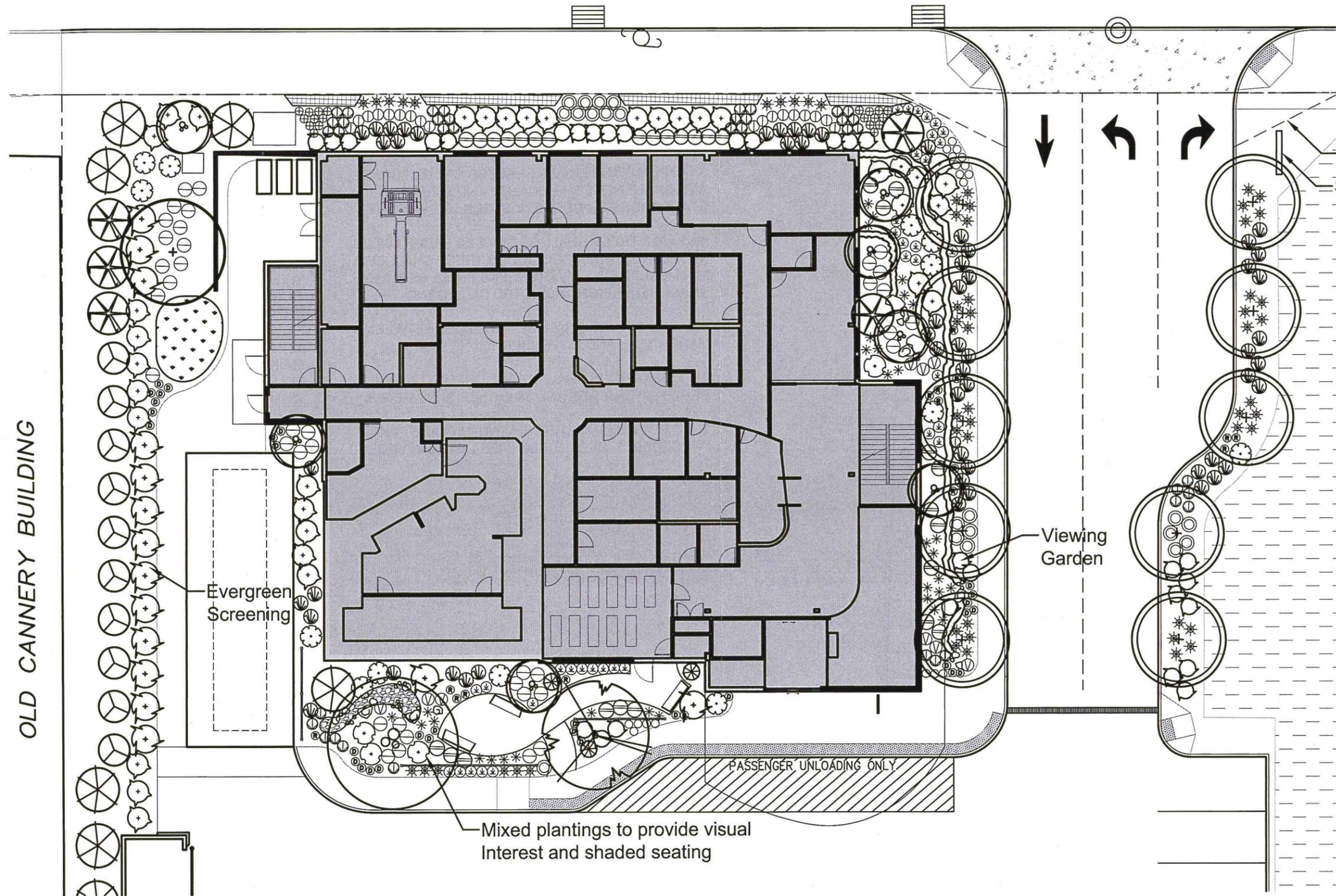


LANDSCAPE NOTES

1. LANDSCAPE CONTRACTOR SHALL SEE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
2. ALL LANDSCAPING SHALL BE INSTALLED ACCORDING TO ACCEPTED BEST INDUSTRY STANDARDS FOR PLANTING PROCEDURES TO INSURE HEALTH AND SURVIVABILITY.
3. PLANT MATERIALS SHALL BE OF HIGH GRADE, AND SHALL MEET THE QUALITY AND SIZE STANDARDS FOR NURSERY STOCK.
4. ALL TREES SHALL BE SINGLE-LEADER UNLESS NOTED IN LEGEND, DECIDUOUS TREES SHALL BE A MINIMUM OF 8' HT, EVERGREENS SHALL BE FULLY BRANCHED, HEIGHT AS NOTED ON LEGEND.
5. PLANTINGS SHALL BE INSTALLED BETWEEN MARCH 15TH AND NOVEMBER 15th.
6. PLANTINGS SHALL BE MULCHED TO A MINIMUM DEPTH OF 2 INCHES AND 18 INCHES IN DIAMETER.
7. PLANT QUANTITIES SHOWN IN PLANS TAKE PRECEDENCE OVER QUANTITIES LISTED IN LEGEND.
8. TREES WILL BE PROPERLY GUYED AND STAKED SO AS TO NOT INTERFERE WITH VEHICULAR OR PEDESTRIAN TRAFFIC.
9. PLANTS INDICATED AS SPECIMEN ARE TO BE HAND SELECTED BY LANDSCAPE ARCHITECT.
10. PARKING TREES REQUIRED:
1 PER 10 PARKING SPACES
58 PARKING SPACES = 6 TREES
PARKING TREES PROVIDED = 13
11. ALL LANDSCAPING SHALL BE MAINTAINED AND KEPT FREE FROM TRASH, NOXIOUS GROWTH, AND WEEDS. UNKEMPT LANDSCAPED AREAS SHALL BE CONSIDERED A NUISANCE AND SHALL BE ENFORCED UNDER APPLICABLE CITY CODE
12. PROPERTY OWNER IS RESPONSIBLE FOR THE MAINTENANCE OF REQUIRED RIGHT-OF-WAY PLANTINGS.
13. LANDSCAPE SHALL COMPLY WITH CITY OF ASTORIA DEVELOPMENT CODE CHAPTER 3.105 AT TIME OF BUILDING PERMIT.

EXCHANGE STREET

OLD CANNERY BUILDING



Evergreen Screening

Viewing Garden

Vision Triangle, typ
Site Sign

Mixed plantings to provide visual interest and shaded seating

PASSENGER UNLOADING ONLY

Note: See Figure 3C for Plant Legend

Columbia Memorial Hospital

Figure 3B - Enlarged Planting Plan

scale: 1" = 20' - 0"

Date: 2016.4.01
Project Number: 326101



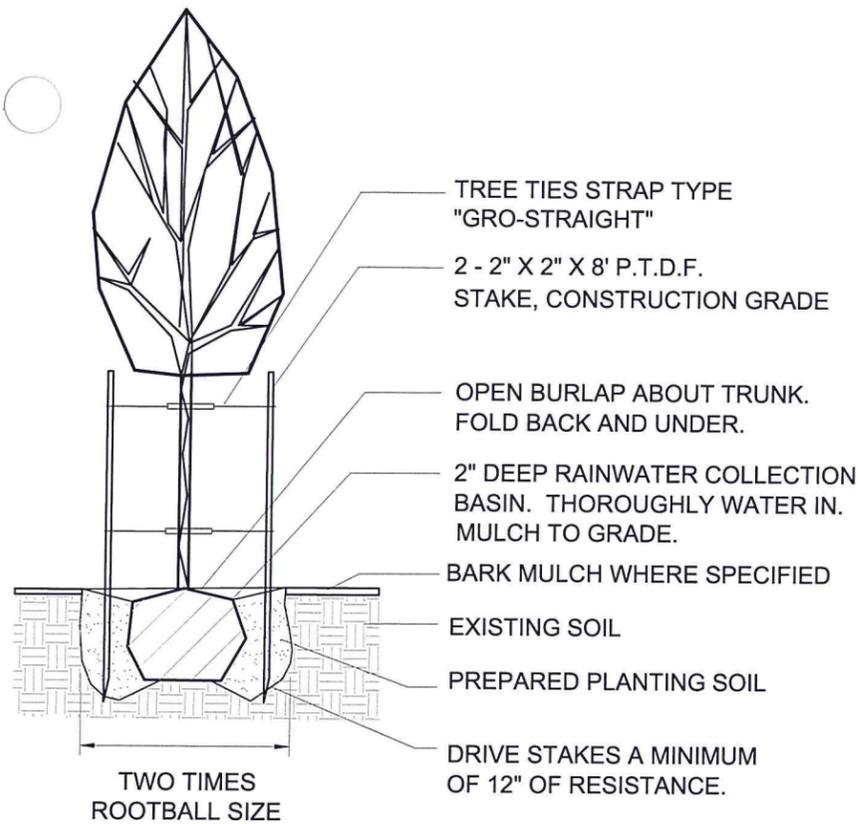
For Review Only Not for Construction

CMH Cancer Center Collaborative



PLANT LEGEND

SYMBOL	NAME	QTY.	SIZE	TYPE	SYMBOL	NAME	QTY.	SIZE
TREES					DECIDUOUS SHRUBS			
	<i>ACER CIRCINATUM</i> Vine Maple	7	8-9' HT.	3 TRUNK MIN.		<i>BERBERIS THUM. 'CRIMSON PIGMY'</i> Crimson Pigmy Barberry	18	12-15"
	<i>ACER RUBRUM 'JFS-KW78'</i> Armstrong Gold Maple	10	2 1/2" CAL.	B&B MATCHING		<i>CORNUS SERICEA 'KELSEYI'</i> Kelsey Red Osier Dogwood	9	12-15"
	<i>ACER PLATANOIDES 'EZESTRA'</i> Easy Street Maple	14	2" CAL. 8' HT. MIN.	B&B		<i>PHYSOCARPUS OPULIFOLIUS 'SEAWARD'</i> Summer Wine® Ninebark	18	5 GAL.
	<i>CERCIDIPHYLLUM JAPONICUM</i> Katsura	1	1.5-2" CAL. 8' HT. MIN.	3 TRUNK MIN.	GRASSES & FERNS			
	<i>CHAMACYPARIS NOOTKATENSIS</i> Alaska Cedar	5	8-9' HT.	B&B		<i>CALAMAGROSTIS A. 'KARL FOERSTER'</i> Karl Foerster's Feather Reed Grass	57	3 GAL.
	<i>PICEA OMORIKA 'RIVERSIDE'</i> Riverside Serbian Spruce	7	8-10' HT.	B&B		<i>CAREX MORROWII 'AUREA VARIEGATA'</i> Variegata Sedge	32	2 GAL.
	<i>PRUNUS x YEDOENSIS 'AKEBONO'</i> Akebono Cherry	1	2 1/2" CAL. 8' HT. MIN.	B&B		<i>HELICTOTRICHON SEMPERVIRENS</i> Blue Oat Grass	94	1 GAL.
	<i>x CUPRESSOCYPARIS LEYLANDII</i> Leyland Cypress	10	8-10' HT.	B&B		<i>PENNISETUM ALPEDUROIDES 'HAMELNS'</i> Hamel Dwarf Fountain Grass	36	2 GAL.
EVERGREEN SHRUBS						<i>POLYSTICHUM MUNITUM</i> Western Sword Fern	30	2 GAL.
	<i>ERICA CARNEA 'SPRINGWOOD WHITE'</i> Springwood White Heath	18	12-15"	CONTAINER	GROUND COVERS & PERENNIALS			
	<i>ERICA x DARLEYENSIS 'KRAMERS RED'</i> Kramers Red Heath	57	12-15"	CONTAINER		<i>HEMEROCALLIS 'STELLA DE ORO'</i> Day Lily	32	1 GAL.
	<i>ESCALLONIA 'COMPAKTA'</i> Compact Escallonia	58	12-15"	CONTAINER		<i>HEUCHERA 'SHANGHAI'</i> Shanghai Coral Bells	EST	1 GAL @18" O.C.
	<i>PIERIS JAPONICA 'MOUNTAIN FIRE'</i> Mountain Fire Pieris	32	7 GAL.	CONTAINER		<i>LAVENDULA x INTERMEDIA 'HIDCOTE GIANT'</i> Hidcote Giant English Lavender	56	1 GAL.
	<i>MAHONIA AQUI. 'COMPACTA'</i> Compact Oregon Grape	38	12-15"	CONTAINER		<i>RUDBECKIA 'EARLY BIRD GOLD'</i> Early Bird Gold Black-eyed Susan	15	1 GAL.
	<i>PICEA ABIES 'NIDIFORMIS'</i> Bird's Nest Spruce	3	21-24"	B&B		<i>SEDUM x 'AUTUMN FIRE'</i> Autumn Fire Stonecrop	20	1 GAL.
	<i>RHODODENDRON 'UNIQUE'</i> Unique Rhododendron	7	24-30"	CONTAINER	STORM WATER FACILITY PLANTINGS			
	<i>VIBURNUM x BURKWOOD</i> Burkwood Viburnum	15	7 GAL.	CONTAINER				



TREE PLANTING DETAIL

SCALE: NTS

Stamp:

Consultant:



Client:
**COLUMBIA
MEMORIAL HOSPITAL**
ASTORIA,
OREGON



**CANCER
CENTER**

Key Plan:

Sheet Title:
**SITE UTILITY
PLAN**

Drawn By:

Date:

April 1, 2016

Revisions:

No.	Revisions	Date

**DESIGN REVIEW
APPLICATION**

Project Number:

131109

Sheet Number:

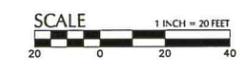
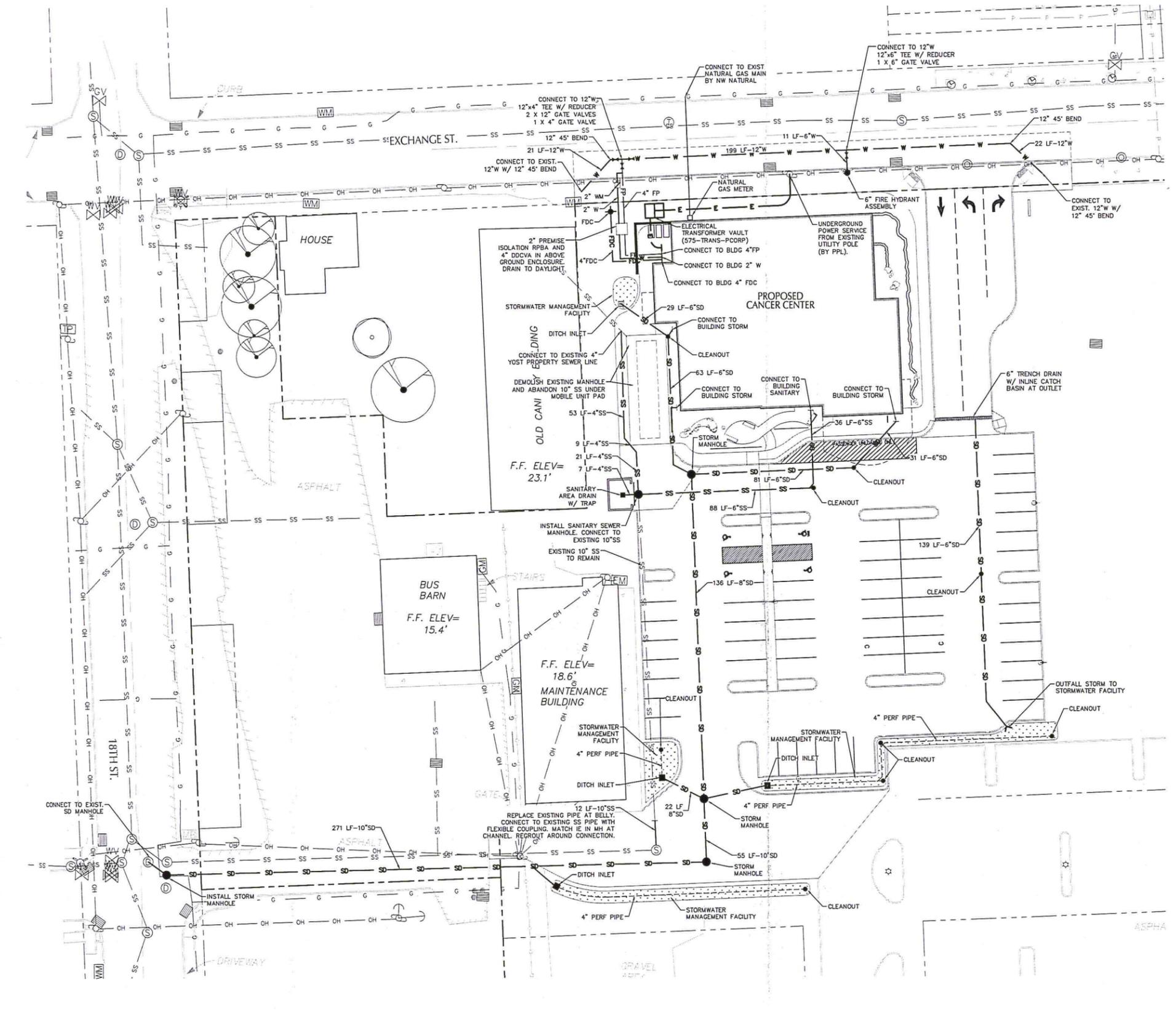
FIGURE 4

SHEET LEGEND

- PROPOSED**
- SD STORM DRAIN
 - FP FIRE PROTECTION
 - W DOMESTIC WATER
 - SS SANITARY SEWER
 - RW RAINWATER HARVESTING (ROOF DRAINAGE)
 - CLEANOUT
 - FIRE HYDRANT
 - AREA DRAIN
 - CATCH BASIN
 - DITCH INLET
 - OUTFALL INTO BASIN
 - STORM MANHOLE

ABBREVIATIONS

- LF LINEAL FEET
- SD STORM DRAIN
- SS SANITARY SEWER
- W WATER
- FP FIRE PROTECTION
- DDCVA DOUBLE DETECTOR CHECK VALVE ASSEMBLY
- RBPA REDUCED BACKFLOW PRESSURE ASSEMBLY
- WM WATER METER



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 PLOT: 4094-FIG 01.dwg Plot Size: 11.00 x 17.00
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Figure 5
BUILDING PERSPECTIVES



Figure 5
BUILDING PERSPECTIVES



Figure 5
BUILDING PERSPECTIVES



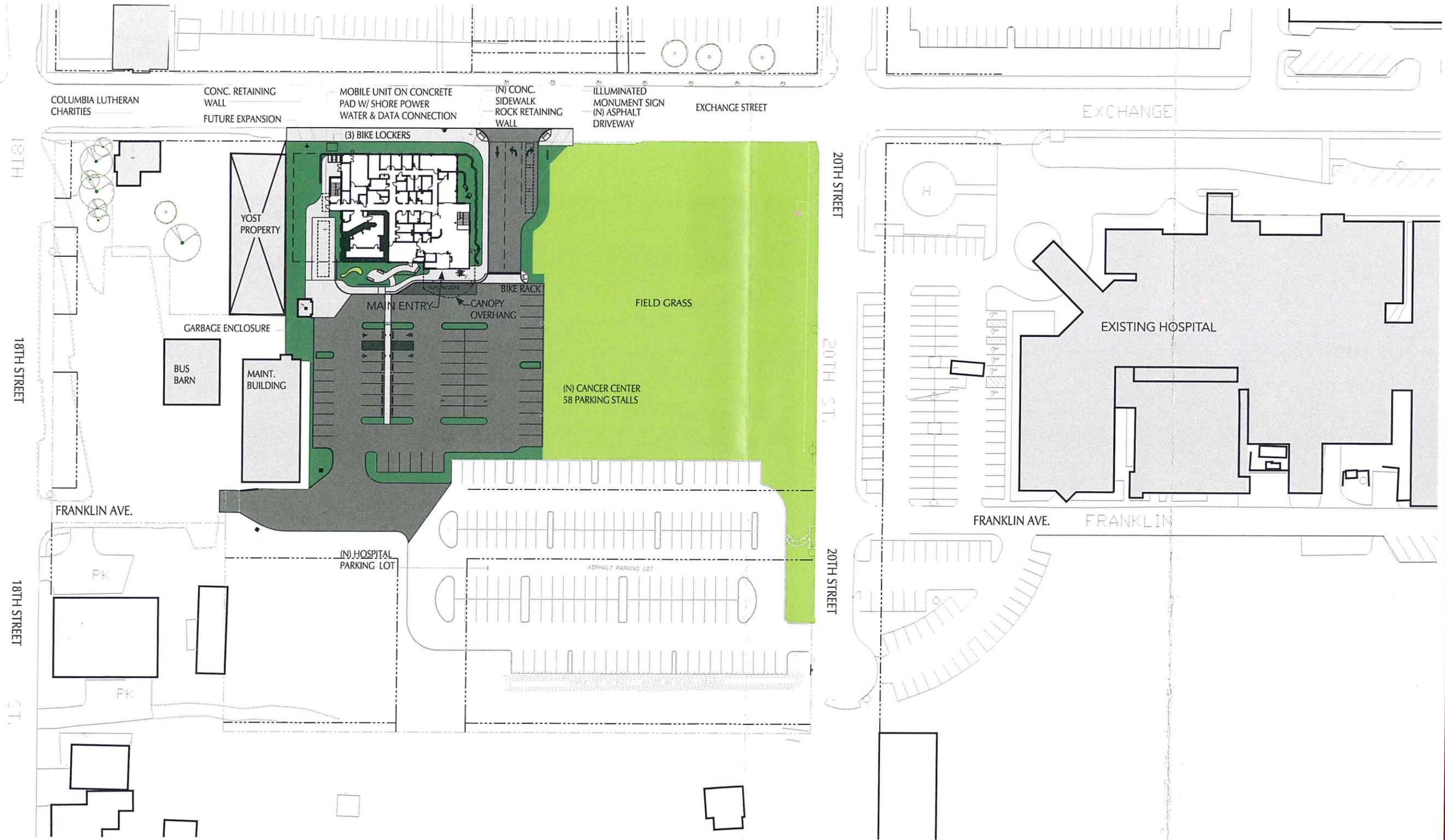
Figure 5
BUILDING PERSPECTIVES



Figure 5
BUILDING PERSPECTIVES

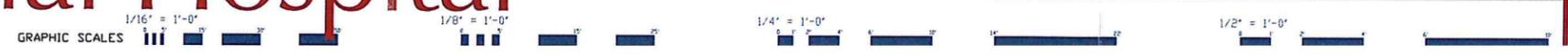


Figure 5
BUILDING PERSPECTIVES



Columbia Memorial Hospital

FIGURE 6 - ARCHITECTURAL SITE PLAN



Date: 6/2/15
Project Number: 140202

FIGURE 7 - DOOR DETAILS

Entry door anodized aluminum automatic sliding door. South entry/exit door from classroom – anodized alum swing door. West exit door from corridor – anodized alum swing door. Similar to the CMH Pavilion Building.

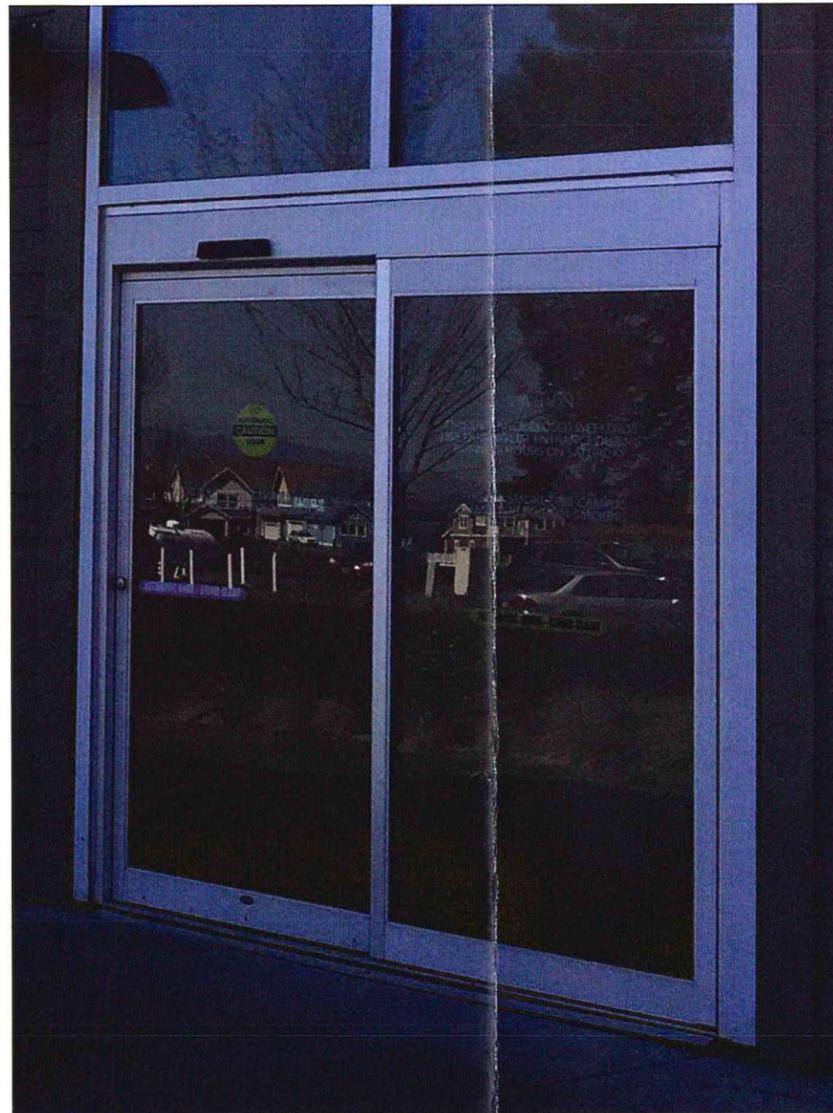


FIGURE 8 - SIDING DETAILS

Vertical metal siding, vertical board and batten fiber cement siding 3" battens – 12" boards. Horizontal fiber cement lap siding. Similar to the CMH Pavilion Building (only vertical metal paneling).



FIGURE 9 - ROOFING DETAILS

Barrel vault over main building – low profile standing seam metal. Similar to the CMH Pavilion Building.



FIGURE 10 - WINDOW DETAILS

Anodized aluminum storefront system. Possible curtainwall on north elevation. All windows are true divided lites. Similar to the CMH Pavilion Building.

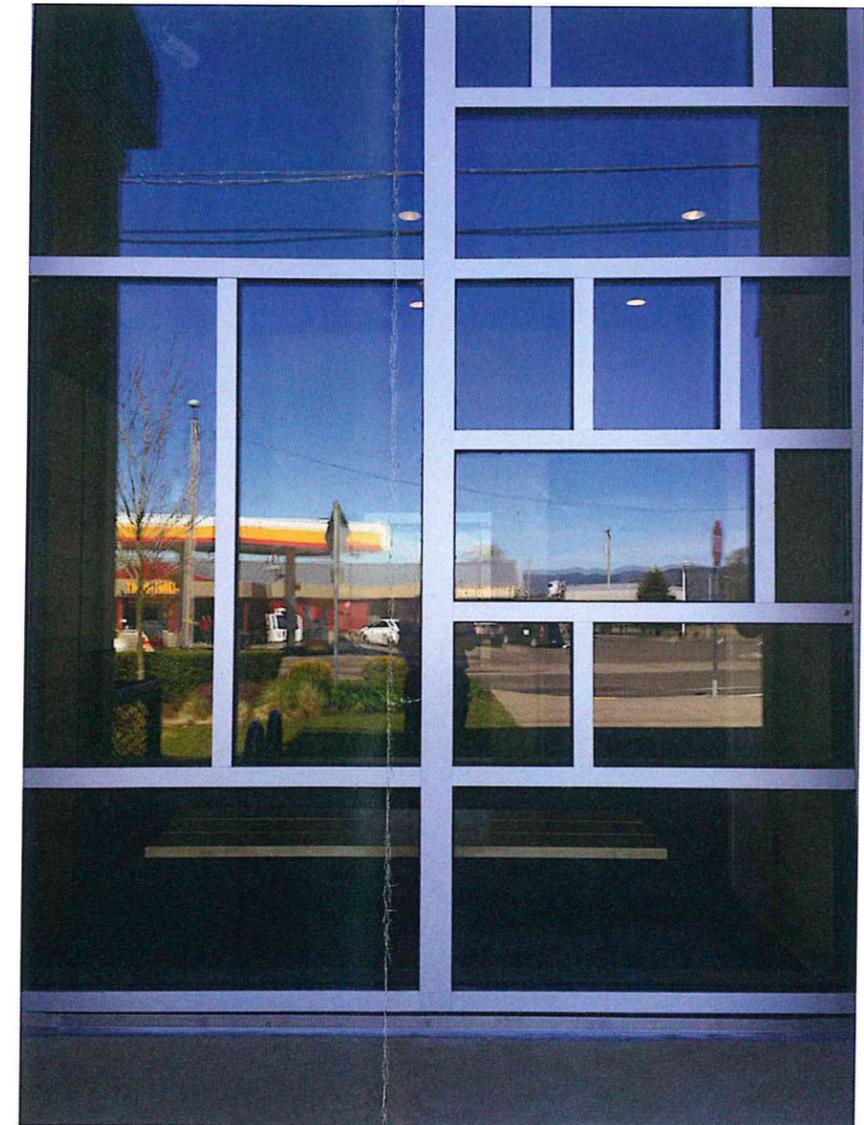


FIGURE 11 - TRIM DETAILS

Trim and weatherproofing details will be similar to the CMH Pavilion Building.



FIGURE 12 - DECORATIVE ELEMENTS DETAILS

Wood eave and canopy ceilings in certain location, decorative wood columns for canopy support and north elevation.



FIGURE 13 - EXISTING CONDITIONS

Pictures of the existing site on John Warren Field.

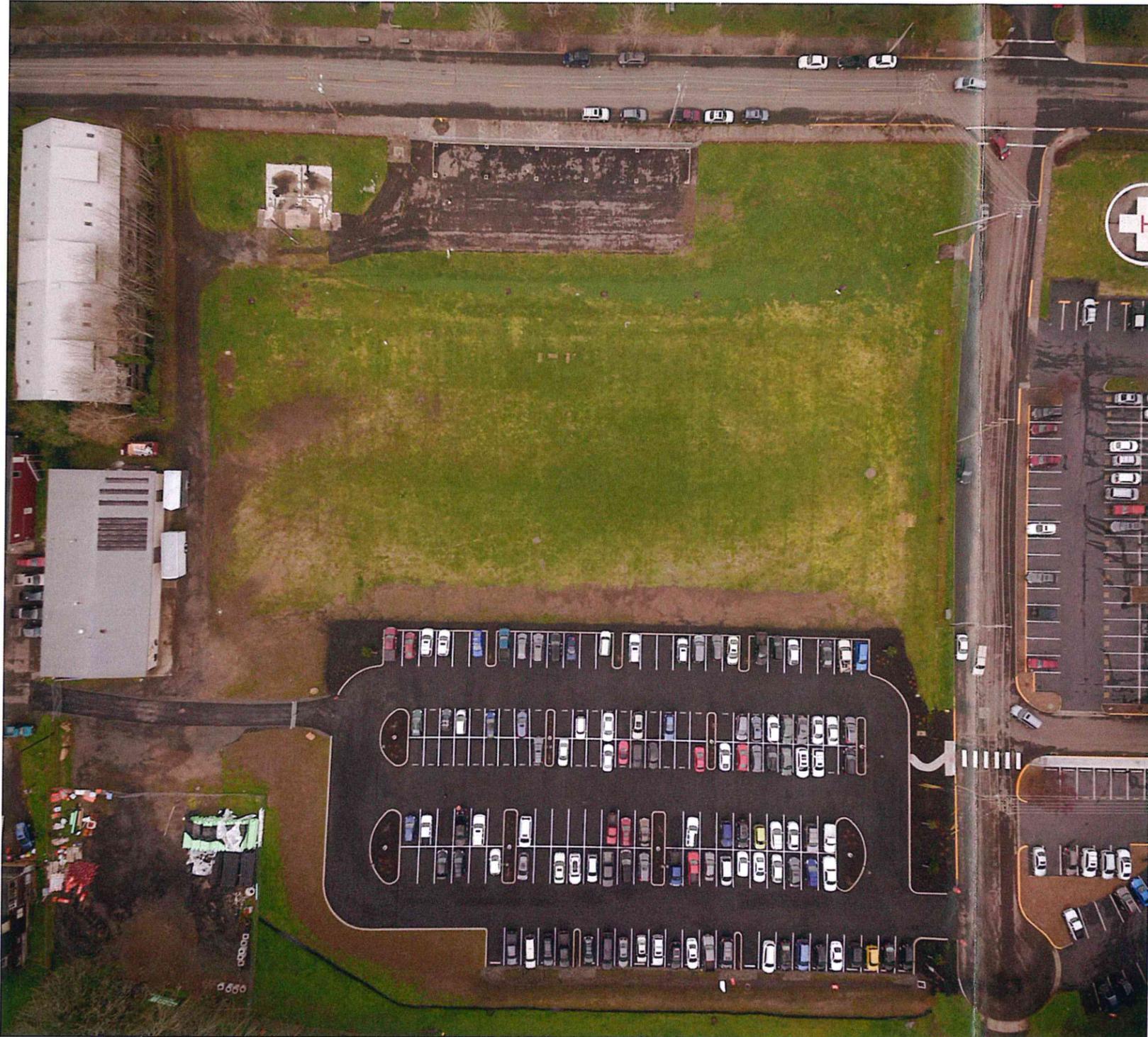
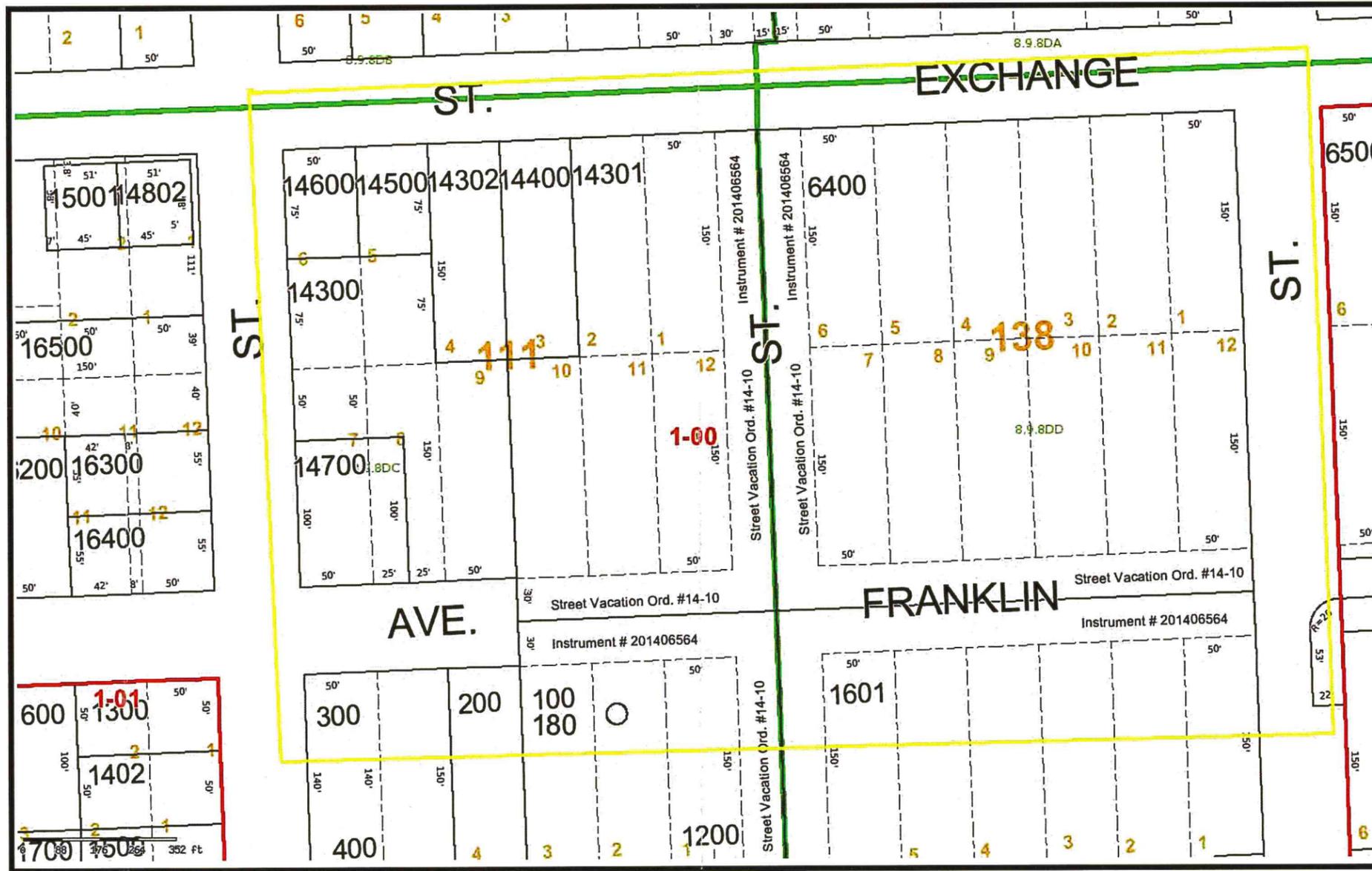


FIGURE 14 - COUNTY TAX MAP

Map



Clatsop County Webmaps

Disclaimer: This map was produced using Clatsop County GIS data. The GIS data is maintained by the County to support its governmental activities. This map should not be used for survey or engineering purposes. The County is not responsible for map errors, omissions, misuse or misinterpretation. Photos may not align with taxlots.

